



Town of Hilton Head Island Regular Design Review Board Meeting

Tuesday, August 14, 2012
1:15 p.m. – Benjamin M. Racusin Council Chambers

AGENDA

As a Courtesy to Others Please Turn Off All Cell Phones and Pagers during the Meeting.

1. **Call to Order**
2. **Roll Call**
3. **Freedom of Information Act Compliance**
Public notification of this meeting has been published, posted, and mailed in compliance with the Freedom of Information Act and the Town of Hilton Head Island requirements
4. **Approval of Agenda**
5. **Approval of Minutes** – Meeting of July 24, 2012
6. **Staff Report**
7. **Board Business**
8. **Unfinished Business**
 - A. Minor External Change
 - 1) DR 120020 – Big Chill Ice
9. **New Business**
 - A. Consideration of Amendment of the previously approved Minutes of the Meeting of March 27, 2012
 - B. Alternation/Additions
 - 1) DR 120021 – Palmetto Bay Marina
 - 2) DR 120022 – Advance Auto Parts
 - 3) DR 120023 – Skull Creek Boathouse
 - C. APL 120002 –North and Trotter sign
10. **Appearance by Citizens**
11. **Adjournment**

Please note that a quorum of Town Council may result if four (4) or more of Town Council members attend this meeting.

Town of Hilton Head Island
Design Review Board
Minutes of the Tuesday, July 24, 2012 Meeting
1:15p.m – Benjamin M. Racusin Council Chambers

DRAFT

Board Members Present: Chairman Scott Sodemann, Vice Chairman Deborah Welch,
Tom Parker, Jennifer Moffett, Galen Smith and Todd Theodore

Board Members Absent: Jake Gartner, Excused

Council Members Present: Bill Ferguson

Town Staff Present: Jennifer Ray, Urban Designer
Richard Spruce, Plans Review Administrator
Kathleen Carlin, Secretary

1. CALL TO ORDER

Chairman Sodemann called the meeting to order at 1:15p.m.

2. ROLL CALL

3. FREEDOM OF INFORMATION ACT COMPLIANCE

4. APPROVAL OF THE AGENDA

The Board **approved** the agenda as presented by general consent.

5. APPROVAL OF THE MINUTES

The Board **approved** the minutes of the July 10, 2012 meeting as presented by general consent.

6. STAFF REPORT

None

7. BOARD BUSINESS

None

8. UNFINISHED BUSINESS

(Mr. Theodore recused himself from review of the following application due to a professional conflict of interest. A Conflict of Interest Form was completed, signed, and attached to the record.)

DR120017 Building Innovations – New Development Final Review

Ms. Ray presented a brief summary of the Conceptual New Development application for the proposed project located at 36 New Orleans Road. The Board approved the Conceptual application for this new design center on June 12, 2012. The two-story building is approximately 3,500 square feet in size. The building fronts New Orleans Road with the rear elevation facing William Hilton Parkway. The new building is adjacent to commercial property on both the east

and the west. Ms. Ray presented an overhead review of the parking area, the elevations, the color palette and the materials, the pervious pavers, the lighting, and the landscaping plan. Access to the site is through a central drive aisle with parking stalls located on each side. Pervious vehicular pavers will be used in the parking area. Decorative pervious pavers will be used at the entrance to the building.

Ms. Ray stated that the building floor plan has not changed from the Conceptual submission. The front and the rear elevations have also not changed since the Conceptual review. Ms. Ray presented the changes to the right elevation and to left elevation. Ms. Ray presented samples of the building's colors and materials all of which are nature blending and appropriate to the Design Guide. Palmetto Electric shoebox light fixtures on a wood pole are proposed to provide lighting from the parking lot to the building during evening hours. Aluminum floodlights for signage have a bronze finish. More plantings are indicated for the entrance and for the building. The landscape plan is appropriate for the site.

The applicant has applied to the Board of Zoning Appeals for a variance to encroach into the buffers. If the Board of Zoning Appeals does not approve the variance request, the applicant may be required to modify the building footprint and or the site plan. The staff recommended that the application be approved as submitted. Following the staff's presentation, Chairman Sodemann requested that the applicants make their presentation.

Mr. Rick Clanton, Architect, and Mr. Trey Griffin, Land Planner, with Wood + Partners, presented statements in support of the application. Mr. Clanton and Mr. Parker discussed the rear elevation. Mr. Parker recommended that the applicant consider adding control joints to the rear elevation to visually help break up the mass a bit. Mr. Parker stated that detail for the trellis is missing from the application. The applicant should submit details for the trellis to the staff for their approval.

The applicants and the Board discussed several issues including the side elevations, the top of the chimney area, and the possible need for additional landscaping at the rear elevation. Additional understory plantings are recommended to help screen the rear elevation. At the completion of the discussion, Chairman Sodemann requested that a motion be made.

Vice Chairman Welch made a motion to **approve** the Final New Development application for Building Innovations as presented. Mr. Parker **seconded** the motion and the motion **passed** with a vote of 5-0-0.

(Mr. Parker recused himself from review of the following application due to a professional conflict of interest. A Conflict of Interest Form was completed, signed, and attached to the record.)

DR 120016 – Sea Tower Beach Park – New Development Final Review

Ms. Ray presented a brief summary of the Conceptual New Development application for the proposed project located at 99 South Sea Pines Drive. The Board approved the Conceptual application for the Tower Beach Park on June 12, 2012. The Sea Pines ARB has also provided preliminary approval. The site plan includes a security shelter, some improved parking, an entry shelter, the beach pavilion, a new system of boardwalks, and minor renovations to the existing restroom building.

As recommended by the Board, the applicant has extended the sidewalk and connected it to the bike trail. The applicant has also added some bike racks for additional bike parking. The applicant has submitted a landscape plan that is appropriate for the area. Site lighting is limited to

two Palmetto Electric shoe box fixtures on wood poles in the parking lot. The applicant has provided details of the trellis swings located along the boardwalk. The trellis swings blend well with the elevations of the building. As recommended by the Board, the architect has added some bracketing to the security station. The footprint and entry shelter have remained largely the same. Some bracketing and detail has been added to the roof elevation. The beach pavilion has stayed the same.

Ms. Ray presented samples of the materials and the colors for the shingles, the guard rails, the windows and doors, and the stucco finish. All colors and materials are nature blending and appropriate to the Design Guide. The staff recommended that the application be approved as submitted. Following the staff's presentation, Chairman Sodemann requested that the applicants make their presentation.

Mr. Jake Lee, Architect, and Mr. Chris Darnell, Land Planner, with JK Tiller & Associates, presented statements in support of the Final application. Mr. Lee presented comments regarding design improvements to the security shelter. Mr. Lee also presented comments regarding the Main Pavilion. In consideration of turtle protection, the facility will not be used after dark, and no lighting will be provided after dark except for some emergency-type lighting that will have no exterior impact. The Board complimented the quality of the project. The Board stated that the additional brackets and the attached bike path are nice additions to the plan. At the completion of the Board's discussion, Chairman Sodemann requested that a motion be made.

Mr. Theodore made a motion to **approve** the Final New Development application for Tower Beach Park as presented. Mr. Smith **seconded** the motion and the motion **passed** with a vote of 5-0-0.

9. NEW BUSINESS

DR120020 – Big Chill Ice - Minor External Change

Ms. Ray introduced the application and stated its location. The applicant is proposing to place a 200 square foot ice vending machine at 123 Mathews Drive. The machine is 9' high x 8'-4" wide x 24' long. The corporate colors for the ice vending machine are bright white with a blue canvas awning.

Ms. Ray presented an overhead visual review of the proposed project, the site, and location of adjacent commercial property. The proposed location for the ice vending machine is highly visible from Mathews Drive. The scale of the machine, the colors, and the materials do not meet the intent of the Design Guide and are foreign to the surrounding architecture and landscaping. If approved, the staff recommends that the color of the machine be changed to match the adjacent building. The ice vending machine should be screened from view. Following the staff's presentation, Chairman Sodemann requested that the applicant make his presentation.

Applicants, Mr. Bubba Gillis and Mr. Patrick Mason, presented statements in support of the application. The applicants and the Board discussed the need to change the white color to match the color of the adjacent building. The Board recommended that the blue awning be changed to green. The machine should be screened from view.

The Board further recommended that the machine be pulled back further from the parking area. The applicant stated his concern with potential damage to the root system of an existing live oak tree. Ms. Ray recommended that staff meet the applicant on site to determine if moving the machine back will cause a problem to the root system of the tree.

The Board stated that they would like to see samples of the proposed colors and materials. The Board would like to see how the awning will look. A revised site plan will also be needed. After conferring with the staff, the applicant decided to **withdraw** today's submission for recommended changes. The applicant will resubmit his application at a later time.

10. APPEARANCE BY CITIZENS

None

11. ADJOURNMENT

The meeting was adjourned at 1:55p.m.

Submitted By:

Approved By:

Kathleen Carlin
Secretary

Scott Sodemann
Chairman

DRAFT



Town of Hilton Head Island
 Community Development Department
 One Town Center Court
 Hilton Head Island, SC 29928
 Phone: 843-341-4757 Fax: 843-842-8908

FOR OFFICIAL USE ONLY
 Date Received: 7/10/12 7/31/12
 Accepted by: DH
 App # DR 120020
 Meeting Date: 7/24/12

Bubba Gillis www.hiltonheadislandsc.gov DBA: The Mailbox & Shipping Center

Applicant/Agent Name: Big Chill Ice Co Company: Big Chill Ice Center
 Mailing Address: 3 Cardinal Ct. #101 City: Hilton Head State: SC Zip: 29926
 Telephone: 843-681-5565 Fax: 689-2860 E-mail: Bubba HHI @ Aul.com
 Project Name: Ice Vending Machine By Chill Ice Project Address: 123 Mathews Drive
 Parcel Number [PIN]: R511 008 000 0226 0000 Town Bus. License # 2173
 Zoning District: _____ Overlay District(s): _____

DESIGN REVIEW BOARD (DR) SUBMITTAL REQUIREMENTS

Digital Submissions may be accepted via e-mail by calling 843-341-4757.

Project Category:

- New Development – Conceptual
- New Development – Final, indicate Project Number
- Alteration/Addition
- Minor External Change

Submittal Requirements for *All* projects:

- Private Architectural Review Board (ARB) Notice of Action (if applicable): When a project is within the jurisdiction of an ARB, the applicant shall submit such ARB's written notice of action per LMO Section 16-3-1004. Submitting an application to the ARB to meet this requirement is the responsibility of the applicant.
- Filing Fee, New Development \$175, Alterations/Additions \$100, Minor External \$50 cash or check made payable to the Town of Hilton Head Island.

Additional Submittal Requirements:

New Development – Conceptual Approval

- A survey (1"=30' minimum scale) of property lines, existing topography and the location of trees meeting the tree protection regulations of Sec. 16-3-405, and if applicable, location of bordering streets, marshes and beaches.
- A site analysis study to include specimen trees, access, significant topography, wetlands, buffers, setbacks, views, orientation and other site features that may influence design.
- A draft written narrative describing the design intent of the project, its goals and objectives and how it reflects the site analysis results.
- Context photographs of neighboring uses and architectural styles.
- Conceptual site plan (to scale) showing proposed location of new structures, parking areas and landscaping.
- Conceptual sketches of primary exterior elevations showing architectural character of the proposed development, materials, colors, shadow lines and landscaping.

Additional Submittal Requirements:

New Development -- Final Approval

- _____ A final written narrative describing how the project conforms with the conceptual approval and design review guidelines of Sec. 16-4-503.
- _____ Final site development plan meeting the requirements of Sec. 16-3-303.F.
- _____ Final site lighting and landscaping plans meeting the requirements of Sec. 16-3-304 and Sec. 16-3-305.
- _____ Final floor plans and elevation drawings (1/8"=1'-0" minimum scale) showing exterior building materials and colors with architectural sections and details to adequately describe the project.
- _____ A color board (11"x17" maximum) containing actual color samples of all exterior finishes, keyed to the elevations, and indicating the manufacturer's name and color designation.
- _____ Any additional information requested by the Design Review Board at the time of concept approval, such as scale model or color renderings, that the Board finds necessary in order to act on a final application.

Additional Submittal Requirements:

Alterations/Additions and Minor External Changes

- A written narrative describing how project conforms to design guidelines of Section 16-4-503.
- Photographs and/or drawings of existing development.
- Drawings of the proposed development - 11"x 17".
- _____ Material/color samples of existing and proposed changes - 8 1/2" X 14" *Maximum*; Stating manufacturer and material name

Note: All application items must be received by the deadline date in order to be reviewed by the DRB per LMO Section 16-3-106.

A representative for each agenda item is strongly encouraged to attend the meeting.

Are there recorded private covenants and/or restrictions that are contrary to, conflict with, or prohibit the proposed request? If yes, a copy of the private covenants and/or restrictions must be submitted with this application. YES NO

To the best of my knowledge, the information on this application and all additional documentation is true, factual, and complete. I hereby agree to abide by all conditions of any approvals granted by the Town of Hilton Head Island. I understand that such conditions shall apply to the subject property only and are a right or obligation transferable by sale.

I further understand that in the event of a State of Emergency due to a Disaster, the review and approval times set forth in the Land Management Ordinance may be suspended.

Bubba Gillis
SIGNATURE

7-30-12
DATE

Narrative for Big Chill Ice Dispenser

Meeting Date: ~~June 24, 2012~~ July 30, 2012

Bubba Gillis owner of Big Chill Ice is requesting approval of a manufactured ice vending machine be placed on 123 Mathews Drive.

The Big Chill Ice vending machine dispenses crushed ice into either 16 pound bags or 20 pounds into coolers for the affordable price of \$1.50. The user selects which ice option he would like, deposits his \$1.50 into the machine, and the ice is dispensed in a 30-second transaction. This means the consumer is getting about 3 times the amount of ice from the vending machine as compared to purchasing ice in a traditional retail store. Like any other vending machine, it may be used 24/7, which should be welcomed by fisherman, shrimpers, and the service crews (landscapers, construction, etc) who have popularized the machines. There are now 2500 of these machines operating throughout the Southeast.

Big Chill Ice is also a "green" and eco-friendly endeavor. Since the ice is made on site in the machine, there is no trucking/hauling the ice all over the area and the ice is fresh and clean.

The overall size of the machine is a total of 200 square feet (8'4" W x 24' L x 9' H). There will be no staff on site as this business is operated as any traditional vending machine is operated and is a "drive-up" type of business.

Thank you,

Bubba Gillis

Paint color for Big Chill Ice Machine



SW 6147 Panda White

Matches Pawn Shop building color

Paint color for Big Chill Ice Machine Awning



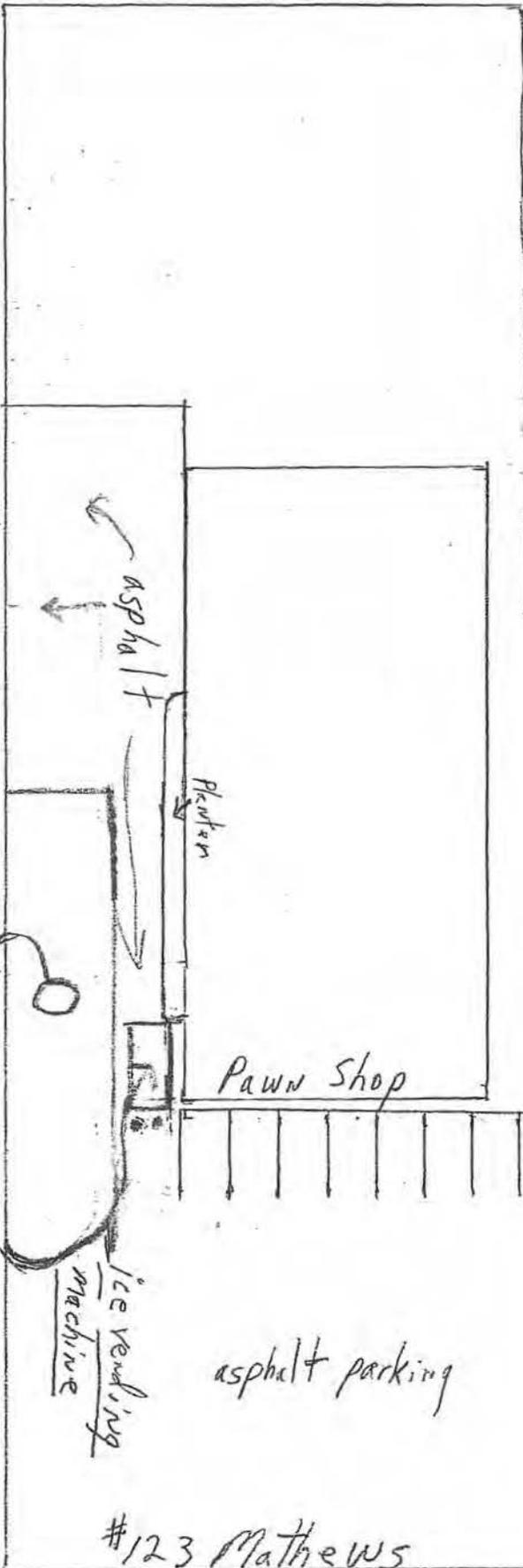
SW 6195 Rock Garden

Matches Pawn Shop Awning color

Commercial
Business

Business

Thompson Street



Auto
Repair

Pawn Shop

Shell
Gas
Station



Propane
sales

restaurant

asphalt parking

#123 Mathews

Mathews Drive





Jennifer, This ice vending machine is 8'-5" wide 12' tall and 24' long





NO
PARKING



NG





NO
PARKING

WE BUY
GOLD
GUNS

DIAMONDS

ROLEX
WATCHES

ELECTRONICS

POWER
TOOLS

Big Money

DODGE

DESPERADO

ATLAS TIRE

PART SHOP OFFER

PART SHOP OFFER

WE BUY
GOLD
GUNS

DIAMONDS

ROLEX
WATCHES

ELECTRONICS

POWER
TOOLS



NO
PARKING





NO
PARKING

NO
PARKING

DODGE

RAM

DESTERADO

MAXX TIRE



NO
PARKING

NO
PARKING



DESIGN TEAM/DRB COMMENT SHEET

*The comments below are staff recommendations to the Design Review Board (DRB)
and do NOT constitute DRB approval or denial.*

PROJECT NAME: Big Chill Ice – MINOR EXTERNAL CHANGE

DRB#: DR 120020

DATE: August 14, 2012

RECOMMENDATION: Approval Approval with Conditions Denial

Based on the DRB recommendations at the July 24th meeting, Staff and the Applicant met on-site to review options for the location of the ice machine. Due to the size and location of the existing tree (a 47” specimen live oak), fencing and landscaping under the canopy are not recommended. The Applicant has agreed to move the ice machine back from its originally proposed location, place the ice machine on the existing asphalt pad approximately 5’ forward of the existing building (aligned with the existing sidewalk in front of the building), to paint the ice machine to match the adjacent building, and to change the awning color to match the adjacent building’s metal roof.

Town of Hilton Head Island
Design Review Board
Minutes of the Tuesday, March 27, 2012 Meeting
1:15p.m – Benjamin M. Racusin Council Chambers

REVISED
DRAFT
8/14/12

Board Members Present: Chairman Todd Theodore, Vice Chairman Scott Sodemann, Jake Gartner, Jennifer Moffett, Tom Parker, Galen Smith and Deborah Welch

Board Members Absent: None

Council Members Present: Mayor Pro Tem Ken Heitzke and Bill Ferguson

Town Staff Present: Mike Roan, Urban Design Administrator
Richard Spruce, Plans Administrator
Rocky Browder, Environmental Planner
Kathleen Carlin, Secretary

1. **CALL TO ORDER**
Chairman Todd Theodore called the meeting to order at 1:15p.m.
2. **ROLL CALL**
3. **FREEDOM OF INFORMATION ACT COMPLIANCE**
4. **APPROVAL OF THE AGENDA**
The agenda was **approved** as presented by general consent.
5. **APPROVAL OF THE MINUTES**
The minutes of the March 13, 2012 meeting were **approved** as presented by general consent.
6. **STAFF REPORT**
None
7. **BOARD BUSINESS**
None
8. **UNFINISHED BUSINESS**
Hilton Head Christian Academy – Practice Gym – DR120004
Mr. Roan stated that the Board is reviewing this project for the fourth time. Mr. Roan described the progress of the application since its last review on March 13th including an update on plan t materials and details regarding the courtyard. The site plan remains unchanged. The applicant was asked to add a little more depth to the gable ends of the building and to bring some of the stucco elements further up into the gable. Altering the stucco finishes on the gable ends and extending the rake has

addressed some of the Board's previous concerns. The roof pitch has also been revised to the extent requested by the Board. Mr. Roan reviewed the elevations and stated that the Natural Resources Division has reviewed and approved the proposed tree removal and mitigation plan. All replacement trees will be field located and approved by Town staff.

The architect for this project, Mr. Greg Beste, was not present at today's meeting. Mr. Matt Skinner, Christian Academy High School Board Member, presented brief comments on behalf of the school.

The Board discussed their concerns with the trellis detail, the pilasters, and the louver stucco treatment. The Board encouraged the applicant to use as much stucco as possible particularly on the north and south elevations. The louver needs to fit into the wall a little better, and more trellis-like detail is recommended. The size of the column for the trellis needs to be made beefier. Following final comments by the Board, Chairman Theodore requested that a motion be made.

Mr. Parker made a motion to approve the Hilton Head Christian Academy – Practice Gym application with the following conditions: (1) the landscape trellises detail should be substantially beefier than what is indicated on architectural; and those trellises are not approved until that is submitted to the staff; (2) the pilasters that are indicated will be 4" – 6" proud of the face of the stucco consistent all the way around the building; (3) the gable end treatments that is a louver stucco treatment the louver shall be integrated into the geometric pattern of the gable end of this building so that it is an architectural element and not just an appliqué that the other trim just seems to randomly run into. The trim shall reflect the profile of the louver however the louver ends up. The louver may be taller or it may be wider, but a different profile than what is shown with a different kind of trim relationship. All of these conditions are to be submitted to the staff for their review. Ms. Welch **seconded** the motion and the motion **passed** with a vote of 7-0-0.

Jenkins Island Cell Tower – DR120007

Mr. Roan stated that all New Development applications require a second (Final) review by the Board. All of the specifications for this application are the same as the Conceptual Approval with the exception of reducing the total height of the tower including lightning rod to 150'-0", per the Board's conditions for Conceptual approval. The staff recommended approval. Following brief comments by the Board, Chairman Theodore requested that a motion be made.

Mr. Smith made a **motion** to **approve** the Jenkins Island Cell Tower – New Development Final application as submitted. Chairman Theodore **seconded** the motion and the motion **passed** with a vote of 7-0-0.

9. NEW BUSINESS

Airport Tree Mitigation – DR120007

Mr. Roan began with a brief history of the application. The primary purpose of today's review is to address the buffer area along Beach City Road. Mr. Roan presented an in-depth overhead review of the application including the applicant's narrative, the existing site conditions, the proposed mitigation plan, and the plant schedule. Mr. Roan stated that the application meets very specific mitigation criteria as established by the Town and the County and as outlined in the applicant's narrative relative to vegetation type, flowering characteristics, and installation size. The Town would like to see as much native vegetation planted in the buffer as possible so that the buffer can re-establish itself. Staff recommended that the following four conditions be attached to approval of the application.

1. All fencing should be black vinyl-clad fencing;
2. The Design Review Board should visit the site following the installation of plant material for approval of the planting. The applicant should be required to supplement additional plant material in any sparse locations as deemed necessary by the Boards.
3. The applicant should consider an irrigation system for the first year following the planting of material in order to ensure the survivability of the new plant materials;
4. An on-going maintenance schedule, beyond the warranty period provided at the installation of plant materials, should be required.

Following the staff's presentation and discussion by the Board, Chairman Theodore requested that the applicant make his presentation. The following individuals spoke on behalf of this application:

Mr. Charles Stearns, Project Manager, CDM Smith, presented statements regarding the historic preservation areas of Mitchelville. The Board and the applicant discussed the mitigation schedule, the recommendation for a temporary irrigation system, and the need for a maintenance contract following the one-year warranty. Chairman Theodore stressed the importance of a temporary irrigation system for the survivability of the trees. This site is already in a parched condition and the summer season will soon compound the problem.

Mr. Gary Kubic, Beaufort County Administrator, presented statements in concern of the tight time line for installation of plant materials. The applicant stated that they are anxious to begin the mitigation planting process as soon as possible. Mr. Kubic and the Board discussed the importance of a maintenance schedule following the installation warranty. The applicant stated that they will hire a maintenance expert for development of a maintenance schedule. The maintenance schedule will be based on the advice of tree experts.

Mr. Paul Andres, Hilton Head Island Airport Director, discussed FAA safety concerns regarding the choices of plant material selected for the area. The FAA considers any vegetation that contains fruit to be an attractive nuisance because of birds being attracted to the fruit. The FAA takes safety concerns with birds very seriously. Mr. Andres recommended that the staff amend the LMO to allow the applicant to plant Bermuda grass in the area.

The Board discussed the statements presented by these speakers and Chairman Theodore discussed the importance of a temporary irrigation system for the site. It is highly unlikely that new trees will survive or be in a healthy condition without the benefit of a temporary irrigation system. A long-term maintenance commitment will also be required beyond the one-year warranty period. The applicant needs to take these commitments very seriously. Vegetation in this area will take more than a year to fill in and take care of itself. Who will maintain the vegetation then? Mr. Andres stated that this will be an operational expense and an appropriate maintenance schedule will be determined by their tree experts.

Chairman Theodore requested public comments and the following residents presented statements: Mr. Edward Tiscornia stated concern with the proposed mitigation plan because of its negative impact on property values and quality of life. The Board should consider a berm to shield the area from noise and fumes. A berm would help to mitigate the area visually and it would help protect the historic area of Mitchellville.

The Board discussed the option of a berm. Chairman Theodore stated that a berm would cause additional existing trees to be removed. In addition, a berm would probably not provide much visual mitigation. Several Board members stated that they are not in favor of a berm.

Mr. James Fisher, resident, stated that the proposed mitigation plan is unacceptable and is a violation of the LMO. More vegetation is needed because the existing 'stalks' are not trees. Adequate irrigation of the plan material will be crucial to its survival.

Residents, Mr. Dan Cleyart, Mrs. Karen Cleyart, and Mr. Fred Keever stated that the proposed mitigation plan is disrespectful of the philosophy and aesthetics of Hilton Head Island. The applicant needs to increase the density of the vegetation. A berm should be considered for the mitigation of noise and fumes.

Chester C. Williams, Esq., appearing on behalf of St. James Baptist Church, stated that the proposed mitigation plan does not comply with the LMO. Mr. Williams stated that LMO Sec. 16-4-403 requires that the airport's adjacent use buffers and adjacent street buffers shall be a minimum of 75-feet in width. The function of buffer areas is to provide aesthetically acceptable visual and spatial separation between adjacent land uses. Mr. Williams also stated that permitted activities in other buffers provide that sanitary sewer service lines are allowed in buffers provided that they are approximately perpendicular to the street right-of-way or the common property line. An equal amount of buffer is required for substitution of the removal of vegetation.

Residents, Mr. David Schwarz, Mr. Don Davis, Ms. Donna Lane, and Ms. Judith Shade all stated concern with the proposed plan based on aesthetics, excessive noise, excessive light, and jet fumes. A sound barrier such as a berm should be considered.

The Board further discussed the idea of a berm with the applicant. Mr. Andres stated that a berm was not considered as an option because a berm requires a change in land shape. A change in land shape is a violation of FAA requirements. In addition, there is no means to pay for a berm.

The Board, the applicant, and the staff discussed the idea of adding an existing 15-ft. sanitary sewer easement to the buffer (for a combined 75-ft. buffer from the property line). Ms. Teri Lewis presented statements regarding the location and legality of adding the 15-ft. easement. Staff discussed the airport buffer and the existing easement. The Board stated that making the buffer 15-feet wider for additional thickness would be a good starting point. The Board and the applicant discussed the planting of materials in the 15-foot easement.

The Board and the staff also discussed the FAA recommendation on the planting material (Bermuda grass) in the open area. The LMO requires that native materials be used. The Board stated concern that Bermuda grass will most likely not be cut or maintained properly.

At the completion of the discussion, the Board reported that they need additional information to reach a decision on this application. The Board stated that they would like to receive clarification on the types of native grasses and magnolia variety proposed. The Board stated that 75-ft. of plantings appears to be a very important issue. With regard to a berm, if a berm is approved, it will need to be placed inside of the easement.

The Board agreed with the importance of performing a visual inspection on site following the initial planting to ensure necessary thickness. The Board stressed that adequate irrigation will be crucial. After one year following installation, a three-year maintenance agreement will be necessary to monitor watering needs, clean up of vines, etc.

Mr. Joshua Gruber, Staff Attorney, presented statements regarding the issue of native grasses. Ms. Teri Lewis presented statements regarding the need for a change to the LMO if the applicant wants to plant only Bermuda grass. Currently the LMO states that the cleared area has to contain native vegetation.

Chairman Theodore stated that aesthetically the native grass option seems to be a good one. The

Board and staff discussed the list of Category I trees (specifically dwarf-sized – appropriate for planting). In conclusion of today’s review, the Board prepared the following list of recommendations for the applicant to consider and bring back a revised plan:

1. The chain link fencing shall be black vinyl-clad fencing.
2. The buffer shall be 75-feet in total, taking the easement into consideration.
3. Look at the best way to get the full 75-foot vegetated buffer along this corridor (without a berm.)
4. In compliance with the LMO, this clear cut area with two grass bid alternates needs to be re-vegetated with native vegetation.
5. The Board recommends a temporary irrigation system be installed and that system be able to stay in place beyond the first year with a maintenance agreement that stays in place for three years beyond that time. Maintenance is critical as precedent for other areas in the future. A long-term maintenance program is very important.
6. After the initial installation is complete, the Design Review Board will conduct a site visit to determine if there are any visual holes. If there are any, then those areas will be supplemented with additional vegetation.
7. The applicant should look into alternatives to the Little Gems – Hybrids of Magnolias that meet the height requirement. Broadleaf evergreen is recommended for variety and texture.

The applicant thanked the Board for their consideration. The Chairman stated that the Board meets every two weeks and that they would like to help the application to move along.

10. APPEARANCE BY CITIZENS

None

11. ADJOURNMENT

The meeting was adjourned at 3:40p.m.

Submitted By:

Approved By:

Kathleen Carlin
Secretary

Todd Theodore
Chairman

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Verbatim Transcript
Design Review Board Meeting
Tuesday, March 27, 2012 1:15p.m.
Benjamin M. Racusin Council Chambers

Town of Hilton Head Island
One Town Center Court
Hilton Head Island, South Carolina 29928

1 CHAIRMAN THEODORE: All right. We'll call
2 to order the Regular Design Review
3 Board meeting for March 27,2012.
4 Kathleen, would you please conduct roll
5 call.

6
7 MS. CARLIN: Mr. Smith?

8
9 MR. GALEN SMITH: Here.

10
11 MS. CARLIN: Ms. Moffett?

12
13 MS. JENNIFER MOFFETT: Here.

14
15 MS. CARLIN: Ms. Welch?

16
17 MS. DEBORAH WELCH: Here.

18
19 MS. CARLIN: Mr. Theodore?

20
21 CHAIRMAN TOOD THEODORE: Here.

22
23 MS. CARLIN: Mr. Sodemann?

24
25 VICE CHAIRMAN SCOTT SODEMANN: Here.

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MS. CARLIN: Mr. Parker?

MR. TOM PARKER: Here.

MS. CARLIN: Mr. Gartner?

MR. JAKE GARTNER: Here.

CHAIRMAN THEODORE: All right, thank you.

Mike, are we compliant with the Freedom
of Information Act?

MR. ROAN: Yes, sir.

CHAIRMAN THEODORE: All right, thank you.

Are there any changes to the agenda?

MR. ROAN: No, no, sir.

CHAIRMAN THEODORE: All right. Moving on,

we'll review the meeting minutes from
March 13th. Are there any comments on
page 1? Page 2? Page 3? Hearing none
the minutes are approved.

1 MR. ROAN: Okay.

2

3 CHAIRMAN THEODORE: Mike, do we have a staff report?

4

5 MR. ROAN: No, sir.

6

7 CHAIRMAN THEODORE: All right. Is there

8 any board business? Hearing none we'll

9 move to --

10

11 MR. ROAN: It is what it is.

12

13 CHAIRMAN THEODORE: Well, before we go to

14 unfinished business let me make an announcement.

15 I know there's probably a number of you out

16 there that would like to speak today, on at least

17 one of the applications so we'll give you the

18 opportunity to speak and when we open that forum

19 there'll be a sign-in sheet and Ms. Carlin,

20 where will that sign-in sheet be?

21

22 MS. CARLIN: It's right there at the podium.

23

24 CHAIRMAN THEODORE: At the podium. So

25 please sign-in and then when you speak

26 state your name and we're going to ask

1 that comments be limited to
2 four minutes and that when somebody
3 speaks, if somebody has duplicated what
4 you were going to say, try to refrain
5 from coming up here so that that person
6 can represent that discussion item.

7 Thank you.

8 Unfinished business, the Hilton
9 Head Christian Academy.

10
11 MR. ROAN: Back and better than ever,
12 Christian Academy. I think this is the fourth
13 time we've looked at this and the site plan remain
14 unchanged. The only comments we had
15 really offered for them to address were
16 the materials to the back and asked them to
17 go to a Wax Myrtle -- which they have done and
18 the rest were dedicated structure.

19 I also note they've integrated
20 that courtyard plan that they looked
21 at, just part of the recorded site plan
22 so that's moving forward now and will
23 be part of their Certificate of
24 Compliance and part of their phasing.

25 We'd asked them to add a little
26

1 more depth to this building and bring
2 some of these stucco
3 elements up into the gable and maybe
4 play with the rake line a little bit,
5 they have done that.

6 Along this north elevation you can
7 see the scoring and the stucco is now
8 brought all the way to the roof line.
9 I think we originally discussed it was
10 maybe to come to these joints and these
11 joints, it looks like it's just shown
12 here in the middle, although it doesn't
13 look bad as shown.

14 The east elevation they've
15 come back and all the elevations are
16 basically the same but they have dashed
17 in with the bleachers and the press box
18 was shown just to show you that really
19 about the only visible spot of this is
20 going to be this end of the east
21 elevation and that presents itself
22 pretty well. You can tell they've
23 played with the rake on the gable end
24 just a little bit here and projected it
25 about, I think it's a foot and-a-half
26

1 if you look at the narrative, it's such
2 a huge gable and it's a large roof
3 you're dealing with some weird
4 proportions if you get them a lot
5 further out than that but they did
6 address that comment.

7 The south elevation they've done
8 the same to address the gable end and
9 the west elevation remains the same.

10 I -- you know, we sort of got on a
11 slippery slope, to begin with, when we
12 let a big metal building in the door
13 and I think they've kind of addressed our
14 comments over and over again to the
15 point where this is an approvable
16 project.

17 I will open it up to the Board for
18 any questions or comments and if
19 anybody is here to represent the
20 applicant the applicant can address
21 those, as well.

22 I do know Mr. Beste's not here
23 today but there are members from the
24 Board to answer any questions that you
25 might have.

26

1 CHAIRMAN THEODORE: All right. Is there
2 anybody that wants to add anymore to --
3 you want to come up?

4
5 Mr. MATT SKINNER: I want to
6 add -- I just want to say I'm here to
7 represent Christian Academy.

8
9 CHAIRMAN THEODORE: Okay. So what Mike
10 has presented, that's covered
11 everything?

12
13 MR. MATT SKINNER: Yes.

14
15 CHAIRMAN THEODORE: All right. Mr.
16 Gartner, do you have any comments?

17
18 MR. GARTNER: Mike, you'd
19 mentioned in the diagram, though, I was
20 still seeing that it's still talking
21 about stucco finish with control
22 joints, do you know have those been
23 changed to pilasters that are part of
24 the building or are they still -

25
26

1 MR. ROAN: They're still labeled as
2 control joists.

3

4 MR. GARTNER: I mean that -- to me
5 that was a big one.

6

7 MR. ROAN: Where they picked up the
8 projection I think is when they came
9 out with this bump-out, you know, and
10 bringing it up into the roof line of
11 the gable, but no, they still remain
12 control joints.

13

14 MR. GARTNER: I mean in our last
15 meeting I thought that was -- that was
16 a -- at least that was a big one, to
17 me, due to the fact that it was going
18 to break up that long expansive
19 building on both sides.

20

21 MR. ROAN: I know we did discuss it.
22 I don't recall a direct -- a directive
23 as like in addition to the applicant
24 although it's an easy fix and part of a
25 motion that can be made.

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MR. GARTNER: Right.

MR. ROAN: I don't think it's something that has to come back again.

MR. GARTNER: I agree.

MR. ROAN: I feel strongly -

MR. GARTNER: I agree.

CHAIRMAN THEODORE: I would add, secondly, Jake, that you're mainly referring to the two long sides, right?

MR. JAKE GARTNER: Correct.

CHAIRMAN THEODORE: That would be of interest, and I think what he demonstrated, too, was that there's bleachers, you know, on one side and then obviously the gap between the adjacent building is pretty much blocking almost the entire other side as well so I just throw that out there.

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MR. GARTNER: No, I understand that, okay. I
mean that was really the only question that I had on
it.

On the elevation, the north
elevation, is there a -- is that break
where the louver is, is that a
different finish or is that just a cut
line for the metal going all the way
across there? Do we know that?

MR. ROAN: Specifically where are you referring
to? I'm sorry.

MR. GARTNER: I'm
referring to the top half of the
building.

MR. ROAN: Oh, the diagonal
over here? I think it's a graphic.
For some reason it's easier to hatch 90
percent of these metals as opposed to
just hatching all of the metal but I
think they're indicating it's metal on
either side of stucco bump-out on both
sides.

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MR. GARTNER: Your thought is that's stucco there where the louver is?

MR. ROAN: It's stucco between where my arrow is here and where my arrow is here and it's metal on either side.

MR. GARTNER: Okay.

CHAIRMAN THEODORE: And that's the same for the other end of the building, as well?

MR. ROAN: Yes, sir.

MR. GARTNER: That's all I have.

CHAIRMAN THEODORE: Mr. Parker?

MR. PARKER: Yeah, I agree with what we talked about the pilasters, they did need to be more than a

1 control joint and the trim detail,
2 that's starting to happen in the center
3 line of the gable, looks great.

4 The louver has absolutely nothing
5 to do with -- the trim is running into
6 it and it's just sort of looks like
7 it's just there. The trim and the
8 louver need to be integrated into some
9 sort of logical pattern, very, very
10 simple to fix but let that be an
11 architectural element that makes some
12 sense instead of just running things
13 into each other, different widths and,
14 you know, that's the right direction
15 just let's finish that detail up and
16 make that element something that will
17 -- I mean that is the most visible site
18 of this thing, let's make it into
19 something not just an appliqué.

20

21 CHAIRMAN THEODORE: All right. Thank you.

22 Mr. Sodemann?

23

24 VICE CHAIRMAN SODEMANN: I agree with what
25 these guys have already said but also I
26 noticed that, you know, we've got this

1 little bump-out now on the north
2 elevation which is -- I'm fine with
3 that, I think that's a good start, but
4 now it seems to -- might be creating a
5 problem with the trellises below, it
6 bumps out about halfway through one of
7 them and so that's going to affect your
8 courtyard plan, as well.

9 I definitely, aside from that, you
10 know, encourage as much stucco as we
11 can get on this thing, particularly on
12 the north and south elevations, and I'm
13 onboard with the pilaster comments, as
14 well. We still haven't really seen any
15 definitive details on the trellises or
16 anything like that but the landscape
17 designer's rendition seemed to be a
18 little bit heavier feel to it, which I
19 think is good, and also I think that
20 louver, you know, if anything, it needs
21 to be lower, it needs to have a littler
22 proportion on the wall and fit into the
23 wall a little bit better. Seems like
24 we're getting there but we're taking
25 baby steps in getting there but I just
26 don't think it's there all the way,

1 yet.

2

3 CHAIRMAN THEODORE: All right. I was
4 trying to remember, didn't we talk with
5 them about bringing more trellis like
6 the detail on that? That there was
7 obviously a difference between what the
8 architectural was showing and then what
9 that little courtyard sketch was
10 showing? That -- for some reason I
11 thought we had talked about that they
12 were going to bring a little bit more
13 --

14 MR. ROAN: Explicitly
15 mentioned it.

16

17 CHAIRMAN THEODORE: Okay, thank you.

18

19 MS. WELCH: I'm feeling
20 like they've come a long way and there
21 are little tiny things that will make a
22 big difference, the column, the size of
23 the columns for the trellis and the
24 other things that people have mentioned
25 here, they're small things but I think
26 they will have a big impact.

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CHAIRMAN THEODORE: All right. Ms.
Moffett?

MS. MOFFETT: I would agree with
everything that's been said, so far. I
think the comments about seeing more
stucco and then the integration and the
louver are ones that stick out for me
because I did have a problem with the
roof pitch and I think, you know, that
might be moving towards making me feel
a little bit better about that but I
just echo everything that's been said,
so far.

CHAIRMAN THEODORE: All right, thank you.
Mr. Smith?

MR. SMITH: I have nothing further to add.

CHAIRMAN THEODORE: Are you -

MR. SMITH: I'm happy with what
I've heard.

1 CHAIRMAN THEODORE: Okay.

2

3 MR. SMITH: Everything's been
4 stated.

5

6 CHAIRMAN THEODORE: Okay. I have the same
7 feelings, as well. I guess we were
8 looking for a little more on this next
9 step hoping that this would, you know,
10 take it to where it needed to be and we
11 seem to get nods from the architect, as
12 well, on what we were asking them to
13 bring back.

14 Does -- sir, would you like to
15 come up and add anymore that -- we seem
16 to be kind of missing some information
17 on here, and I don't know if you want us
18 to take a vote or?

19

20 MR. SKINNER: I can't
21 give you any more information other than that.

22 I know that Greg and I have talked and
23 he mentioned that they were pilasters.

24 He didn't say that they're expansion
25 joints so that's my understanding is
26 that he had added those back in.

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CHAIRMAN THEODORE: Would you be comfortable if we make a motion and because -- you know, that we can try to formulate a motion to prove this but we'll -- we're going to make a suggestion that these be pilasters?

MR. SKINNER: Absolutely. I'm very comfortable with that.

CHAIRMAN THEODORE: And would you mind stating your name just for the record.

MR. SKINNER: Matt Skinner. Matt Skinner.

CHAIRMAN THEODORE: Okay. Thank you, Matt. Tom or Scott, do you think you could come up with a motion?

MR. ROAN: Can I just offer something? We're in a pattern here where I think the property owner wants to do the absolute bare minimum

1 permissible to build this thing and
2 it's been openly expressed, this is a
3 project driven on value.

4

5 CHAIRMAN THEODORE: Right.

6

7 MR. ROAN: And you
8 keep making recommendations for
9 approval and we kind of keep getting
10 closer and closer and never get there.
11 It's in your motion you'd like to see
12 explicitly what would come back, we
13 approve this and I can then ensure that
14 gets done to a level of satisfaction --
15 you know, where I think a level of
16 satisfaction would be.

17

18 CHAIRMAN THEODORE: Okay.

19

20 MR. ROAN: But it's
21 pretty clear what it is. It's the -
22 of the trellis post, it's the
23 pilasters, it's the detailing of the
24 joints and the louver -

25

26 CHAIRMAN THEODORE: Right.

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MR. ROAN: -- at the top of the gable. I think if you give them a laundry list and this is -- we can move on but I hate to make this thing come back four or five times.

CHAIRMAN THEODORE: This is number four.

MR. ROAN: This is number four. If we go to five, then we all get a free sub.

CHAIRMAN THEODORE: All right.

MR. PARKER: Well, for the record we're a Design Review Board not a design board, you know, we're not here to design these projects, but with that said I'll try to go ahead and make a motion that we approve this project with the following conditions; the landscape trellises detail be submitted that is substantially beefier than what is indicated on architectural and those trellises aren't approved until that is submitted to staff.

That the pilasters that are

1 indicated there will be four to six
2 inches proud of the face of the stucco
3
4 consistent all the way around the
5 building. That the gable end
6 treatments that is a louver stucco
7 treatment, the louver be integrated
8 into the geometric pattern of the end
9 -- the gable end of this building so
10 that it is an architectural element and
11 not just an applique that the other
12 trim just seems to randomly run into,
13 the trim will reflect the profile of
14 the louver, however the louver ends up,
15 the louver may be taller, it may be
16 wider, but a different profile than
17 what's shown with different kind of
18 trim relationship and that will be
19 submitted, also, for staff review.

20

21 CHAIRMAN THEODORE: All right.

22

23 UNIDENTIFIED SPEAKER: I'll second
24 that motion.

25

26 CHAIRMAN THEODORE: All right. Any

1 further discussion? All in favor?

2 All right. Approved. Thank you.

3 Our next unfinished business is
4 the Jenkins Island Cell Tower.

5

6 MR. ROAN: This is a
7 formality in the lowest sense of the
8 word. This is a new development. It
9 has to come back twice. We approved it
10 the last time with the condition that
11 that be -- new cell tower vertical
12 elements not to exceed 150 feet
13 inclusive of the lightning rods. That
14 trips -- the FAA requirement you have
15 to put strobe lights on it. They've
16 updated that on their drawings, staff
17 recommends approval.

18

19 CHAIRMAN THEODORE: All right. Thank you.

20 Any discussion? You want to make a
21 motion?

22

23 MR. SMITH: I make a
24 motion we approve the cell tower.

25

26

1 CHAIRMAN THEODORE: As submitted?

2

3 UNIDENTIFIED SPEAKER: As submitted.

4

5 CHAIRMAN THEODORE: I'll second that. Any

6 discussion? All in favor? All right.

7 That completes the unfinished business.

8 Now onto new business, the airport tree

9 mitigation.

10

11

12 MR. ROAN: Okay. As

13 you may have noticed, riding up and

14 down Beach City Road, there's been a

15 significant change to the landscape

16 adjacent to the airport. We're not

17 going to -- well, this is all per FAA

18 requirements for heights of trees.

19 In the landing zone trees need to

20 be on 1 to 37 pitch along the center

21 line of the runway and then a 1 to 7

22 pitch laterally moving away from this

23 zone.

24 The first phase of this project we

25 went over a few years ago was in the

26 south end of the airport around the

1 Wachovia Bank and the IHOP and things
2 like that and this is significantly
3 more aggressive.

4 Along the runway it's been --
5 we're looking at, now, primarily
6 addressing the buffer on Beach City
7 Road. We're not here to discuss FAA
8 policy. We're not here to discuss, you
9 know, the why's of the safety rules.
10 The buffer's been cleared, the trees
11 have been topped that's what's out
12 there now.

13 This is the mitigation plan as
14 presented by the County and the
15 airport. There were very specific
16 requirements about what could be
17 planted. Obviously anything that is
18 planted can't grow to a mature height
19 that would violate that 1 to 37 slope
20 in one direction and 1 to 7 in the
21 other.

22
23 MS. TERI LEWIS: The slope is 34 to 1.

24
25 MR. MIKE ROAN: 1 to 34, 1
26 to 34. The second was the Town wanted

1 to see native material, we'd kind of
2 like to see that buffer reestablish
3 itself as part of the native landscape.
4 The County wanted anything that goes in
5 the ground to not have any berries or
6 potentially be wildlife habitat,
7 in the case that there would be birds
8 flying around the airport that might
9 jump into the jets, so that establish a
10 palette, sort of a mutually agreeable
11 palette of the plants that can go in this
12 area.

13 Everything that you'll see
14 presented today meets that requirement
15 in terms of mature height, in terms of
16 native material, in terms of potential
17 habitat and is what is presented to you
18 today. I'm going to just cut right to
19 the chase. I'm going to bring you a
20 couple photographs up, first.

21 You're going to see a plan, it's
22 pretty aggressive what they're putting
23 back into the ground. There are
24 mentions of existing trees in the
25 field. This is what's out there now
26 along Beach City Road. The runway

1 extends off the right side of the page
2 but everything past the end of the
3 runway has been clearcut to the ground.
4 Trees have been removed and underbrush
5 has been removed. Trees that were
6 selected by an arborist have been
7 topped at a predetermined height and
8 they've been established that these
9 will revegetate and eventually mature
10 to a height that the FAA does not find
11 objectionable but when you see an
12 existing tree on your plan right now
13 this is what they're referring to, sort
14 of the remnants of what was in place
15 before, a couple other photographs.
16 This is another look at it and these
17 are -

18
19 CHAIRMAN THEODORE: And these
20 trees will be maintained? I mean like
21 every ten years they'll come back and
22 --

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24 MR. MIKE ROAN: They're on
25 a ten-year pruning cycle, budget
26 permitting.

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CHAIRMAN THEODORE: And the trees that were there before had been pruned like this in the past?

MR. ROAN: There are some that have been pruned in the past and had revegetated, you know, there's about probably 1,700 trees that remain here. I don't have a visual inventory of every one of them but I think that's a pretty good character study of what you see there. That's, I would say, pretty indicative of what's out in the field.

You could see the airport in the background. Behind it there's a chain link fence in the back that sort of indicates the end of what had been cleared before. This would accommodate all the FAA requirements and any future master plan in the airport right now.

And what the applicant has come back with is as follows: here's the existing runway you can see the center line demarcated in increments in

1 ten along the center line.
2 Here's the cone as outlined. Here's Beach City
3 Road. This is the subject area, the
4 buffer along Beach City Road that comes
5 back around and then terminates on the
6 adjacent property line to -- on Dillon
7 Road. Here's the historic church on
8 the corner of Beach City and Dillon,
9 just as a point of reference.

10 There is a large wetland at the
11 end of this zone but what we're reviewing today,
12 the extent is located from here extends along
13 Beach City Road wrapping all the way around Dillon
14 to this point along the cone.

15

16 CHAIRMAN THEODORE: And this represents all
17 of the airport property?

18

19 MR. ROAN: This is all
20 on airport property, that is correct.
21 The cone is, under FAA jurisdiction, is
22 indicated by this dashed line here and
23 this dashed line here. That integrates
24 with the street buffer and the adjacent
25 property buffer along with this solid
26 gray hatch along there and this is what we're

1 looking at today.

2 A little closer of the area is
3 this area right through here. Here's
4 the church. Here's Dillon Road.
5 Here's Beach City Road and then the
6 cone is shown there as a solid line.
7 Here's the existing end of the runway.
8 There's a chain link fence in place
9 here. Some wetland demarcation.

10 And then here we get to the plant
11 schedule as proposed. They've decided
12 on four different tree types, Little
13 Gem Magnolias, Wax Myrtles, Yaupon
14 Hollies and Southern Red Cedar.

15 Now, the Southern Red Cedars, upon
16 maturity, are really pushing that 34 to
17 1 slope so they're all sort of down in
18 the Dillon Road end. At maturity
19 you're still hitting that slope for
20 takeoff. Everything else is indicated
21 on eight-foot centers. You can tell
22 it's very dense as represented. I'm
23 going to zoom in just a little bit so
24 you can see some of what is indicated is
25 existing trees here, you know, this
26 symbol right here and this was in

1 everybody's package, you know, this was
2 pretty well delineated here, here, and
3 here. Those are all indicative of the
4 stumps or the stalks that you were shown.
5 It's a fairly dense plan. I'm going to
6 be honest with you, it's kind of a
7 kitchen sink plan. They're throwing a
8 lot of materials at us; eight feet on
9 center. They're specifying ten-foot
10 tall material or two-inch caliper
11 height at DBH, diameter at breast height,
12 upon installation, all of which are
13 pretty hearty materials.

14 Staff would recommend approval as
15 submitted but I would like you to maybe
16 consider the following: first there is a
17 new chain link that's installed and I just
18 want to review the standard rider that we use for
19 for any chain link fence that it be blacked
20 -- or black vinyl chain link fence, I just
21 want to get that out of the way.

1 The second is that upon
2 installation that the condition be in
3 place that the Design Review Board
4 visit the site and if there's just any
5 egregious areas that are, you know,
6 sparse with site lines in or out of the
7 airport that they might be reinforced.

8 The third is to consider any sort
9 of irrigation in the first year to
10 ensure the survivability of a good deal
11 of this material.

12 And the last is just the ongoing
13 maintenance of the material and for
14 a -- you know, maybe a time period to
15 be determined by you all, you know,
16 it's one thing to throw these in the
17 ground and keep them wet for the first
18 few weeks and -- you know, and hitting
19 hope and see what comes up, making sure that
20 they don't get, you know, encased by
21 vines and just don't survive. And it'd
22 be a shame, frankly, not only for the
23 buffering of it but just for the
24 viability of the money you're throwing

1 at it, just all this money out in
2 field. This will be an expensive
3 project as presented to throw all this
4 money into the field and have it not
5 survive even a year -- there is an alternate
6 -- this area is to be grassed. There's
7 two alternates, in the bid sheet one's to be
8 Bermuda, Bermuda grass, and there's --
9 for native marsh grasses like Muhly
10 grass, Spartina grass, cord grass,
11 things like that.

12 There's a -- the language in the
13 LMO dictates that it be native grass
14 which would then subsequently dictate
15 that it be the Spartina and the Muhly
16 grass; just keep that in mind in it's
17 review.

18 We've hit the tip of the iceberg.
19 There's been a lot of tread taken off
20 the tire, again, getting to this point. I can
21 assure you that there's going to be
22 public comments and ideas brought to
23 the table on this so I'll open it to
24 the Board, I'll open it to the

1 applicant moving forward, but those
2 are staff's recommendations as
3 conditions for approval, and I turn it
4 over to y'all but just please keep in
5 mind -

6
7 CHAIRMAN THEODORE: Go ahead.

8
9 MR. ROAN: This is point A, this is where we are, today,
10 all right?

11
12 CHAIRMAN THEODORE: Let me -- because I
13 was going to ask about this just so
14 everybody knows, these are trees that
15 are in that 75 feet buffer -

16
17 MR. ROAN: Correct.

18
19 CHAIRMAN THEODORE: -- that's in the
20 perimeter?

21
22 MR. ROAN: Correct.

23
24 CHAIRMAN THEODORE: And these are the
25 trees that are closest to the existing
26

1
2 end of the runway that are topped at
3 the lowest -

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5 MR. ROAN: Correct.

6
7 CHAIRMAN THEODORE: -- elevation?

8
9 MR. ROAN: Correct.

10
11 CHAIRMAN THEODORE: And that as you get
12 back to that wetland trees are topped
13 but they're up at a higher point so
14 they look more like trees as you get
15 further away?

16
17 MR. ROAN: Yeah, just
18 -- I'm not trying to patronize anybody
19 but I mean if you just took a string on
20 a 1 to 37 slope, that's the head's line
21 that's out there right now.

22 Just a reminder that this is where
23 we are today. We're not looking back
24 on policies that got us to
25 here. Here's where we're starting.
26 We're trying to determine if the buffer

1 that's proposed is sufficient to buffer
2 the airport.

3

4 CHAIRMAN THEODORE: And then in addition
5 the proposed tree replanting mitigation
6 will take place amongst these -

7

8 MR. ROAN: Amongst these trees.

9

10 CHAIRMAN THEODORE: -- these trees.

11

12 MR. ROAN: All of
13 those trees that are shown in that
14 photograph are indicated by the
15 existing trees marks -- that were in
16 the packages that went out.

17 Do keep in mind there's a sewer
18 easement that runs right through the
19 middle of this, that's sort of just
20 another monkey wrench in the toolbox as
21 it were but it's going to have sort of
22 a gap there, it may be sort of a Trojan
23 horse to have it benefit so it's going
24 to make it easier to access this for
25 maintenance or watering, too, so maybe
26 it's a blessing in disguise but I did

1 want to raise that point.

2

3 I will open it to the Board and to
4 the applicant and to anybody who might
5 have any questions or comments to make.

6

7 CHAIRMAN THEODORE: Is the applicant
8 present and would they like -- sir,
9 would you like to come up and add
10 anymore to what Mr. Roan has -

11

12 MR. PAUL ANDRES: I would prefer to turn it
13 over to our consultant -- that developed
14 these plans and he can talk
15 specifically to these plans and
16 answer -

17

18 CHAIRMAN THEODORE: All right. Well have
19 him come on up, please. Please state
20 your full name for the record.

21

22 MR. STEARNS: Charles Stearns with
23 CDM Smith.

24

25 CHAIRMAN THEODORE: All right, Charles.
26 Would you like to add anymore to Mr.

1 Roan's presentation?

2

3 MR. STEARNS: Yeah. There were a
4 couple -- there's an issue that this is
5 a historic -

6

7 MR. SMITH: Would you
8 get closer to the mic, please?

9

10 MR. STEARNS: This is in the
11 Mitchelville historic area extent,
12 it's not actually within HRP boundary
13 for that, but it is in that historic
14 extent and so we're currently the
15 sponsoring agency which is -- the FAA
16 is dealing with the State Historic
17 Preservation Office on those issues for
18 plantback and all the plantback as you
19 saw is within the extents of the
20 buffers as defined in the LMO; that's
21 where we wanted the most dense planting
22 to visually screen this area from the
23 roadways and from public view.

24 There will be no plantback in
25 wetlands because we can't do that under
26 the Corps of Engineer's guidelines but

1 initially when we -- also, initially,
2 when this project was developed, SHPO
3 would not allow excavation of stumps so
4 the entire area that was cleared, which
5 is basically the cross hatched area as
6 shown on that drawing, is this area
7 here back to the buffer line, all this
8 area; that area that was cleared we
9 could only grind stumps down to the
10 surface of the ground.

11

12 CHAIRMAN THEODORE: Okay.

13

14 MR. STEARNS: So we can't use
15 non-tilling methods to plantback because of the
16 stumps and the root mats but we are
17 proposing sprigging of grass in that
18 area; that's really all I have to add
19 to the presentation.

20

21 CHAIRMAN THEODORE: The Historic
22 Preservation Officer is just going to
23 look into any potential conflicts of
24 planting -

25

26 MR. STEARNS: Correct.

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CHAIRMAN THEODORE: -- the vegetation on
top of potentially historic -

MR. STEARNS: Correct, yes, sir.
The FAA and the State Historic
Preservation Office are in dialogue
right now over the issue to determine
whether what the SHPO requirements
will be when we do the tree planting
and it's primarily associated with the tree
planting because of the excavation.

CHAIRMAN THEODORE: All right. With the
Town's recommendations is there any
issue, concerns that you have with
their conditions for approval?

MR. STEARNS: No, sir.

THE COURT: Okay, all right. Do you mind standing
up here as we discuss
this and if we have any questions -

MR. STEARNS: Sure.

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CHAIRMAN THEODORE: -- we can ask them?

All right. Mr. Gartner?

MR. GARTNER: I actually don't have a lot of comments. Initially I understand there's probably going to be a lot of comments from the crowd but, you know, Mike's -- Mike's thoughts for staff for approval on conditions makes a lot of sense to me at this juncture.

CHAIRMAN THEODORE: All right, thank you.

Mr. Parker?

MR. PARKER: I agree with that.

CHAIRMAN THEODORE: All right. Mr.

Sodemann?

VICE CHAIRMAN SODEMANN: Yeah, realizing that this is a Design Review Board and not -- we're not discussing safety issues and things like that I'm in line with that, as well.

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CHAIRMAN THEODORE: All right. Ms. Welch?

MS. WELCH: No, I'll reserve any comments for after looking forward to hearing people speak.

CHAIRMAN THEODORE: All right, thank you. Ms. Moffett?

MS. MOFFETT: I will do the same.

CHAIRMAN THEODORE: All right - thank you. I had a few questions for you if you don't mind if I ask. The -- I notice in the tree planting detail are you going stake every one of these trees?

MR. STEARNS: Yes, sir, we typically do because they're small caliper and they're ten-feet tall initially so when they mature they'll be much larger and those stakes and wires can be removed so -

1 CHAIRMAN THEODORE: Okay. Because that
2 raises -- you know, that's a pretty
3 extensive planting job to -- it's very
4 important to maintain those and make
5 sure, in the end, that they don't
6 become a detriment to the growth of the
7 vegetation.

8
9 MR. STEARNS: Well, we
10 can reinforce the specification.
11 Currently we're requiring the planting
12 contractor to come back and review the
13 area once a month for a year and make
14 sure that he sufficiently waters it and
15 files a report on what he observed and
16 we'll also have representatives of the
17 airport keeping track of that issue.

18
19 CHAIRMAN THEODORE: All right. And how do
20 you propose to handle the irrigation of
21 these -- of this plant material?

22
23 MR. STEARNS: Well,
24 currently we don't have any irrigation,
25 temporary irrigation setup in there
26 other than requiring watering by the

1 contractor so he would have to bring
2 water to the site.

3

4 CHAIRMAN THEODORE: Have you considered
5 temporary irrigation that might sit on
6 the ground?

7

8 MR. STEARNS: No, we have
9 not. These are all, once established,
10 drought tolerant, but we have not
11 considered temporary irrigation.

12

13 VICE CHAIRMAN SODEMANN: As part of
14 the contract is that the installer has
15 to come back for a year's period of
16 time and regularly irrigate -

17

18 MR. STEARNS: That's
19 correct.

20

21 VICE CHAIRMAN SODEMANN: -- that
22 entire area?

23

24 CHAIRMAN THEODORE: And you're going to
25 require the contractor provide a year
26 warranty on the growth of the life of

1 the plant material?

2

3 MR. STEARNS: Sir?

4

5 CHAIRMAN THEODORE : You're going to
6 require, as part of the contract, that
7 the installer guarantee the plants for
8 a year -

9

10 MR. STEARNS: Yes, sir.

11

12 CHAIRMAN THEODORE: -- at a time?

13

14 MR. STEARNS: Yes sir, One-year
15 warranty is required.

16

17 CHAIRMAN THEODORE: All right. And as
18 part of that you're going to require
19 them to take care of the maintenance
20 for a one-year period of time, as well?

21

22 MR. STEARNS: Yes, sir,
23 that is correct.

24

25 CHAIRMAN THEODORE: Have you considered
26 the fact that it's going to take a good

1 bit of water to keep these trees alive
2 for the first year then how's that
3 going to be handled?
4

5 MR. STEARNS : We have, we
6 have thought about it and it is going
7 to be a major issue for the contractor
8 and if he wants to value engineer a
9 proposal to temporary
10 irrigate rather than coming back and
11 individually water the plants otherwise
12 then we would be amenable to review the
13 proposal to do that.
14

15 CHAIRMAN THEODORE: Okay, all right. I think that's
16 all my questions so we're going to open this to the public so I
17 appreciate your input -
18

19 MR. STEARNS: Yes, sir.
20

21 CHAIRMAN THEODORE: All right.
22

23 MR. STEARNS: You want me
24 to stand by here or?
25

26 CHAIRMAN THEODORE: No, you can go back

1 and sit down. Are you representing the
2 applicant, sir?

3

4 MR. KUBIC: Yes, I'm the County
5 Administrator I just wanted make a few comments -

6

7 CHAIRMAN THEODORE: Would you mind coming
8 up? Please state your name.

9

10 MR. KUBIC: My name is Gary Kubic.
11 I'm the Beaufort County Administrator,
12 and I'd like to thank you for the
13 opportunity to speak. I just want to
14 add a couple of things as far as my
15 responsibility and role in the process.

16 We are very anxious to go to bid
17 on this process simply because I am not
18 a forester nor do I know how to plant
19 things and make them grow, but I am
20 making some assumptions that these
21 trees here have optimum periods where
22 it's a better likelihood that they will
23 take and grow and I'm assuming that
24 would be in a less stressed period
25 where the temperatures would be more
26 conducive for tree growth; that being

1 the fall, I'm not sure, but I am
2 positive that through our consultant
3 this program will be presented and go
4 through the FAA requirements and as
5 soon as that is complete we will go
6 through our bid procedure and then go
7 through committee with council and then
8 go to council for formal approval.

9 I believe, from memory, the amount
10 of money that we want to reinvest in
11 these buffer areas is around 400 to
12 500,000. I did want to tell Design
13 Review and I heard that you didn't want
14 to talk about safety concerns, but I did
15 want to mention that I received a
16 letter from the FAA concerning the
17 types of grass in the clear area, it
18 was from the Director of the Atlanta
19 Regional District, Scott Serritt. He had
20 indicated that the choices and, you
21 know, we are a citizen of the Town of
22 Hilton Head, we operate the airport,
23 we're in the County, so we are subject
24 to the jurisdictional requirements of
25 the Town which we fully intend to meet
26 but he suggested that the type of

1 vegetation currently in the LMO have a
2 greater likelihood of attracting birds
3 and therefore the assumption of risk
4 that we assume as operators of the
5 airport is greater because of that
6 potential conflict so I did write the
7 Mayor and the Town Manager requesting
8 the opportunity that if the Town saw a
9 chance to amend the LMO to allow just
10 basic grass in the area and giving us
11 that choice just to reduce that
12 assumption of risk. I haven't heard
13 back from the Town and I'm not sure
14 where that decision is in their
15 processes, nevertheless, if it is
16 decided that they don't go forward with
17 finding vegetation or a plant that does
18 not -- that is not an attractive
19 nuisance for birds and other animals
20 then we will proceed with that and
21 then, you know, my fear and my
22 requirement, as a County Administrator,
23 is to make sure that we're all notified
24 that we have been given an indication
25 from the FAA that we are going to
26 create a greater assumption of risk

1 than a less assumption of risk and if,
2 unfortunately, injury or death occurs
3 with an accident and a plane then I'm
4 assuming that the Town and the County
5 will jointly share in that potential
6 settlement or litigation that follows;
7 and so it's a serious matter, it's not
8 something that we take lightly.

9 I have been given the joint
10 resolution. The decision has been made
11 by the elected officials for me to
12 operate the airport and I just wanted
13 to make you aware of that process.

14 I also would like to thank you.
15 If this meets with your approval I
16 wanted to indicate to you that I am
17 going forward as rapidly as I can to
18 get it before council so that we can
19 get a plan and get the buffer back to
20 where we want it to be. Thank you very
21 much.

22
23 CHAIRMAN THEODORE: If I can ask you a
24 Question?

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26 MR. KUBIC: Sure.

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CHAIRMAN THEODORE: When you talked about the concern about the -- especially I guess the grass area.

MR. KUBIC: Uh-huh.

CHAIRMAN THEODORE: Is that the concern is the fruit of what potential native plant material would have that would attract birds to that area, was that the concern?

MR. KUBIC: Yeah. Apparently the FAA folks view those types of plants as an attractive nuisance.

CHAIRMAN THEODORE: Have they come up with recommendations for other native species to consider?

MR. KUBIC: They did and I'm not a grass expert, but I think it's just plain Bermuda grass.

CHAIRMAN THEODORE: So they don't have an option, they're just saying it's

1 Bermuda grass or nothing else would
2 satisfy?

3
4 MR. KUBIC: And it's the FAA so
5 you have to understand that they're
6 going to tell you or tell us that
7 that's what they prefer and therefore
8 there are no "attractive nuisances"
9 with a Bermuda grass.

10
11 CHAIRMAN THEODORE: Do you know, just by
12 chance, how frequent the grass is mowed
13 around the airport right now; on what
14 kind of regular basis?

15
16 MR. KUBIC: Oh, I have no idea.
17 You could ask my airport manager, but I
18 don't know how frequently we cut the
19 grass.

20
21 CHAIRMAN THEODORE: Yeah.

22
23 MR. KUBIC: I cut mine about once
24 a week.

25
26 CHAIRMAN THEODORE: I know but there's

1 been tendencies to let them grow -

2

3

4 MR. KUBIC: I know.

5

6 CHAIRMAN THEODORE: -- every six months or
7 something like that so I'm just kind of
8 curious but whatever goes over here in
9 this cleared area because you haven't
10 been able to grade it it's the natural
11 topography that's there?

12

13 MR. KUBIC: It's my understanding
14 that because of the archeological
15 requirement and I believe, in part, the
16 Town LMO requirement that we are not
17 permitted to level, if that's what
18 you're asking -

19

20 CHAIRMAN THEODORE: Right.

21

22 MR. KUBIC: -- that area and so

23

24 I'm awaiting the final determination
25 from the archeological survey to see
26 what our next step is as a result of

1 whatever their findings are.

2

3

4 CHAIRMAN THEODORE: But the way it stands,
5 right now, they would go in by hand and
6 plug this Bermuda grass and then mow
7 over the existing topography and in
8 addition there's root stumps and
9 everything else that's being cut at
10 grade -

11

12 MR. KUBIC: Right.

13

14 CHAIRMAN THEODORE: -- all there in place?

15

16 MR. KUBIC: That's correct. One
17 of the questions I did ask the Town at
18 the time is if you could furnish me
19 with the requirement the plantings that
20 are specified in the LMO and identify
21 those that we could cut down to about
22 eight inches and they did that.

23

24 CHAIRMAN THEODORE: Uh-huh.

25

26 MR. KUBIC: I just don't know, from memory, what those

1 specimens are.

2

3

4 CHAIRMAN THEODORE: Okay, all right.

5

6 VICE CHAIRMAN: So the process, from
7 here, is still is you still have to go
8 back through the FAA to get their
9 approval what we've done here or see if
10 they're --

11

12 MR. KUBIC: I'm assuming that all of the
13 preliminary work and the work done by
14 the consultant is done in association
15 with the FAA requirements and so
16 basically to ensure that the funding is
17 not in jeopardy for the buffer
18 planting, that there's a step where
19 they say everything's fine, proceed,
20 you qualify for the FAA grant and I, as
21 Administrator, want to get that
22 assurance that we have the money
23 coming.

24

25

26

I also sent a letter to County
Council indicating that I expect that
if I find a situation where I'm in this

1 process and I need additional funds to
2 complete the process according to town
3 requirements, county requirements and
4 neighbor requirements that, you know,
5 is within the reasonable factor that I
6 fully expect County Council to give me
7 the money so that I complete the job
8 the way you want it.

9

10 CHAIRMAN THEODORE: Yeah, the -- this is a
11 significant replanting program and it's
12 fairly large vegetation, as well. Do
13 you have an idea of what the estimated
14 cost is for this?

15

16 MR. KUBIC: I have been told that
17 it -- well, the best way to gauge it is
18 to get the actual bid.

19

20 CHAIRMAN THEODORE: Uh-huh.

21

22 MR. KUBIC: But I'm assuming it'll
23 be somewhere in the 400 to 500,000
24 range.

25

26 CHAIRMAN THEODORE: With that said and

1 being out there and just seeing, today,
2 how parched that land is, it seems like
3 it would be worthwhile to consider a
4 temporary irrigation system to get the
5 material reestablished, to help the
6 trees that have been pruned severely to
7 reestablish themselves, as well, and
8 just as a steward of, you know, what
9 it's going to take to cost, you know,
10 to plant these things that it would be
11 worthwhile to consider that and how you
12 get water to these trees over this
13 large area is an effort.

14 Have you -- has the airport talked
15 with, you know, where would the water
16 come from? You know, is it going to be
17 loaded in trucks and brought over there
18 or, you know, has there been
19 consideration for maybe a surface
20 temporary irrigation system and would
21 that plug into a water supply that the
22 County has with the airport or would
23 the town or, you know, PSD? Has there
24 been any of those kind of discussions
25 of how that would about taken care of?

26

1 MR. KUBIC: I can't answer that. I
2 can just simply tell you I don't have
3 any opposition to ensuring that we
4 provide whatever is necessary to
5 facilitate healthy trees that are
6 there. I don't know what that means.
7 I do know that I've made the commitment
8 to proceed and am asking, you know,
9 obviously what you see now people are
10 responding to.

11 The situation for me is how do I
12 take the present condition and get it
13 to a point where it begins to change
14 and it becomes more acceptable and it's
15 the buffer. If watering improves that
16 capability then I'm -- I have no
17 objection to it. Why would I want not
18 to water the trees?

19
20 CHAIRMAN THEODORE: Good, good. And
21 that's my opinion, as well.

22
23 MR. KUBIC: Yes.

24
25 CHAIRMAN THEODORE: What about, you know,
26 in this case it is such a large area of

1 replanting, it's -- you know, this
2 vegetation is going to take more than a
3 year to fill in and take care of
4 itself; what are you looking at as a
5 maintenance program that after the
6 contractor's done? The first year,
7 who's going to maintain this and what
8 type of maintenance program will this
9 --

10 MR. KUBIC: Well, the main --
11 well, the way I would look at it is it
12 would be an operational expense within
13 the airport. The appropriate
14 maintenance would be determined by
15 people who are experts in tree
16 management, they would set up a program
17 for us. We could either -- I don't
18 want to necessarily bring it in-house,
19 I probably would prefer to create a
20 professional service contract for that,
21 that way I can create performance
22 measurement standards and have standard
23 of reporting as to what was done on
24 what days and where they were at so I'm
25 assuming that as soon as we get past
26 the warranty, whatever we develop in

1 the bid specification for the initial
2 planting and the requirements by that
3 landscaper to give us what we want,
4 when we assume whatever that
5 appropriate period of time is the
6 responsibility to maintain that would
7 be the course of action.

8
9 CHAIRMAN THEODORE: Good, good. So you'd
10 want to develop, through the
11 airport and County relationship, is to
12 develop a -- some type of maintenance
13 program.

14
15 MR. KUBIC: Oh absolutely.

16
17 CHAIRMAN THEODORE: This is going to be
18 larger than what you have right now?

19
20 MR. KUBIC: Well, and I'm spending
21 our tax dollars via the -- you know,
22 most people say it's the federal
23 government paying it but we -- it's our
24 money.

25
26 CHAIRMAN THEODORE: Right.

1
2 MR. KUBIC: And so I'm not in
3 favor of going into a program and only
4 having 50 percent resolved, that's not
5 acceptable.

6
7 CHAIRMAN THEODORE: Good. I've heard some
8 concerns on the other end of the
9 airport where there had been some tree
10 pruning, that there's vines and some
11 things that are growing up in those
12 areas and I think those are places
13 where you actually want to go back and
14 bring those into conformance, as well,
15 so, you know, that's an example of
16 where, you know, more of a maintenance
17 focus could have been done and now that
18 you have this larger area that is
19 really -- you know, for all these
20 people who are out concerned that make
21 sure that even a year from now, you
22 know, it's being properly maintained.

23
24 MR. KUBIC: You know, I'm a
25 realist and I operate and manage in a
26 very practical way and I know that a

1 lot of times when I say something or
2 make a decision there are 140,000
3 people in the community that either
4 agree or disagree with everything that
5 I do.

6 In the past perhaps the
7 relationship between the County and the
8 Town and maybe to a degree my
9 involvement could have been more
10 hands-on, as an Administrator, but I do
11 manage a large operation, that's in the
12 past.

13 My concern is and I really -- and
14 I -- I've been saying in the paper, I
15 don't know how to create a solution
16 that can blend a runway with a
17 residential neighborhood ever to the
18 total satisfaction of all parties.

19
20 CHAIRMAN THEODORE: Right.

21
22 MR. KUBIC: My solution, in my
23 mind, as a manager, is that it is
24 therefore critical that everybody
25 understand exactly what we're doing and
26 we have, I believe, various parties:

1 Town, the County and the residents.
2 And in the process you will have
3 disagreement, you could have
4 litigation, you could have all kind of
5 variables involved in it but ultimately
6 at the end of the day if the airport is
7 going to be operational there will be
8 the runway and there will be
9 residential neighborhoods, so the
10 buffer, this process, the very next
11 step, whatever you require and whatever
12 I need to do as a County Administrator
13 I'm going to do and I have prepared,
14 through letters to councilmen saying,
15 'Look, I'm going to do this right. I'm
16 going to follow the rules even when I
17 think the assumption of risk is
18 something I would prefer not to do and
19 I want to get it going'. And I've said
20 a couple of times my concern is that
21 the next step people will not
22 understand that, if it is in the fall.
23 It's already April.

24
25 CHAIRMAN THEODORE: Right.
26

1 MR. KUBIC: Now I don't have a
2 large timeline so if it takes a month
3 to go through this process, another
4 month of FAA review and a 90-day bid
5 process I'm pushing it to get to
6 September and I'm assuming that the
7 number of specimens that we are going
8 to propose be put in the ground you've
9 got logistics with whoever is the
10 successful bidder getting the product
11 in here, making sure it's right, so
12 we're in a tight frame.

13

14 CHAIRMAN THEODORE: Right.

15

16 MR. KUBIC: And I'm hoping that we
17 exchange a little bit of trust in the
18 process and I know that's a --
19 sometimes a quantum leap but I'm
20 sincere in the fact that I know what I
21 have out there now and I want to change
22 it as quickly as I possibly can and I
23 need your help, and with that I'd just
24
25 like to say, thank you for your time
26 and I'll sit down and allow the public

1 to comment.

2

3

4 CHAIRMAN THEODORE: Thank you. All right.

5 We'll open the floor to the public. Is

6 there anybody that would like to speak

7 as a citizen on this project? Sir,

8 please come forward. Please sign in

9 and state your name.

10

11 MR. ED TISCORNIA: Long name so give

12 me a second here. My name is Ed Tiscornia and

13 I'm a member of the Palmetto Hall Owners

14 Association Board of Directors. Thank

15 you for giving me this opportunity to

16 speak to the Hilton Head Design Review

17 Board this afternoon.

18 I'm sure all of you are aware that

19 the property owners in Palmetto Hall

20 are extremely interested in the ongoing

21 environmental destruction on the airport property,

22 it's impact on all neighboring property values

23 and our quality of life in Hilton Head Island.

24 The eventual airport tree

25 mitigation of the environmental

26 disaster that currently exists is

1 something on which we wish our voices
2 to be heard and to offer our input.

3 Our airport community and our
4 committee has met and reviewed in
5 detail the CDM plan you are reviewing
6 today.

7 We'd like to offer the following
8 comments: first, we do not feel the
9 proposed mitigation meets the needs of
10 our community because it does not
11 effectively hide or conceal the current
12 environmental disaster. The plan does
13 not effectively mitigate, in our
14 opinion, the increased noise from the
15 excessive tree trimming and will only
16 get worse in the future with runway
17 expansions and greater jet activity.

18 Based on research by members of
19 our airport committee we believe that
20 the only effective mitigation would
21 include an earthen berm design that
22 would be planted with the vegetation
23 spelled out in the Hilton Head's Land
24 Management Ordinance. This type of
25 design has effectively been used on
26 Hilton Head to shield 278 from the

1 residential developments of Windmill
2 Harbor, Long Cove and Wexford.

3 A berm design would also eliminate
4 the need to disturb any priceless
5 Mitchelville archeological artifacts
6 contained below the current ground
7 level. This is not the case with the
8 proposed CDM plan.

9 We also do not believe the plan as
10 presented is either timely or cost
11 efficient -- excuse me, I have a little
12 bit of a cold -- because it would be
13 done before the off-airport property
14 tree trimming has been completed per
15 the Town's ordinance.

16 Per the ordinance the County and
17 the Town are required to work together to
18 develop a landscape plan to meet
19 mitigation requirements based on tree
20 removal documented by the arborist for
21 both off and on airport property.
22 Until all trimming has been completed
23 off the airport property a mitigation
24 plan would not be complete and
25 therefore not cost effective in our
26 judgment because it would require a

1 two-step operation; first to mitigate
2 what you're talking about today and
3 then to come in and again mitigate the
4 tree trimming that's going to be done
5 on off-airport property.

6 I want to thank you again for the
7 opportunity to address the Design
8 Review Board. We hope the Board will
9 keep in mind the need to maintain or
10 improve the world class environmental
11 jewel that Hilton Head has become known
12 for around the world.

13 We are sure if Charles Fraser
14 were alive today and had to drive down
15 Beach City and Dillon Roads he would be
16 here joining us in this appeal. We
17 believe working together and with a
18 little effort we can accomplish a
19 win/win result for Palmetto Hall and
20 all of the residents and visitors to
21 Hilton Head Island. In the end this is
22 really our collective responsibility,
23 isn't it? Thank you.

24
25 CHAIRMAN THEODORE: Before you go let me
26 ask you a question.

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MR. TISCORNIA: Yes, sir.

CHAIRMAN THEODORE: This berm that you're talking about, what's the purpose of the berm, is it -

MR. TISCORNIA: The purpose of the berm is twofold: A, number one would be noise mitigation.

CHAIRMAN THEODORE: Now, when you say that the planes are already -- they've already taken off and they're up above -

MR. TISCORNIA: A lot of -- based on the information that we've gathered a lot of the noise is basically after the plane is on the ground and basically we are going into reverse engine noise, for example, so a lot of the noise impact that we hear is not necessarily when the aircraft is off the ground but -

1 CHAIRMAN THEODORE: Isn't that the case
2 that they're further down the runway,
3 though, when they're throwing their
4 reverse thrusters on and all of that,
5 that the berms would be sort of in the
6 wrong place to be buffering that noise?

7
8 MR. TISCORNIA: Well not
9 necessarily because the -- noise
10 travels -- I'm not a noise expert.

11
12 CHAIRMAN THEODORE: I'm just -

13
14 MR. TISCORNIA: I understand. I'm
15 not a noise engineer but when the
16 aircraft is on the ground and the noise
17 is created, the decibel is created, it
18 travels, you know, in a straight
19 direction and the berm, if you will,
20 would shield and has shielded

1 effectively in other airport designs
2 and situations, there's a berm -- I
3 think there are berms going into the
4 Savannah Airport, as I recall. These
5 devices have been effectively used to
6 mitigate the noise levels at ground
7 level, residential ground levels around
8 airports.

9
10 CHAIRMAN THEODORE: Okay. So it's a noise
11 and visual buffer.

12
13 MR. TISCORNIA: That is correct, two-fold.

14
15 CHAIRMAN THEODORE: Okay.

16
17 MR. TISCORNIA: That is correct.
18 And plus the fact that once we get a
19 berm there with a -- you know, just
20 like if you drive by Windmill Harbor or
21 if you go through the back road, for
22 example, going into the Home Depot, I
23 don't know what that development is
24 there, there's about a four and-a-half
25 or a five foot berm, you turn that
26

1 corner to go into that side road you
2 can't see any of that development
3 behind there, so I think it become --
4 it's our opinion that that becomes a
5 much more effective approach to dealing
6 with this I'm going to use my word
7 'atrocious' that we currently have. You
8 already saw the photographs of it,
9 okay.

10 I've been coming to Hilton Head
11 Island since 1975, '76 and have had
12 many many friends and people come down and
13 join us and the one thing that people
14 have always said is how wonderful
15 Hilton Head Island is in terms of its
16 development. I mean if you look at
17 Wal-Mart or McDonald's or whatever and
18 if you turn and go down Beach City Road
19 today it is absolutely, in my view,
20 unacceptable.

21
22 CHAIRMAN THEODORE: All right. Great.
23 Very well.

24
25 MR. SODEMANN: I supposed
26

1 you would propose to put the berm just
2 along Beach City Road there. About how
3 much space does that take up, width
4 wise? I'm familiar with Windmill
5 Harbor, obviously, and I see there's a
6 sewer easement, there too, as well,
7 and I suspect that you wouldn't be
8 allowed to build anything on top of
9 that sewer easement for whatever -

10
11 MR. TISCORNIA: Again, I'm not a
12 civil engineer.

13
14 MR. SODEMANN: Right.

15
16 MR. TISCORNIA: But I don't
17 know -- if you look at the spacing in
18 there, for example, I'm not so sure
19 that a berm would maybe not necessarily
20 require less lineal space away from
21 Beach City Road, it might be able to be
22 accomplished between the easement,
23 okay, that you're requiring, if you
24 will, for the sewer and Beach City
25 Road, okay. I don't know the answer to
26 that question but I think it's

1 something that, certainly from a design
2 engineering point of view or civil
3 engineering point of view, that it's
4 something that ought to at least seriously
5 considered and looked at.

6
7 CHAIRMAN THEODORE: And you would consider
8 the trees that have been topped that
9 the intent is that those are to regrow
10 branches and, you know, bush out and
11 become a medium size tree? I've seen
12 some examples out there of where the
13 trees were topped and there was even a
14 pine that regrew new vertical growth.
15 If you do a berm you would have to take
16 those trees out that they have saved in
17 there in order to allow a berm to fill
18 in so you would rather see a berm
19 versus keeping the existing trees that
20 are in that buffer zone?

21
22 MR. TISCORNIA: That's the opinion
23 of our airport committee, yes.

24
25 CHAIRMAN THEODORE: Okay. I just wanted
26 to clarify that.

1 MR. TISCORNIA: Yes, correct.

2

3 CHAIRMAN THEODORE: All right.

4

5 MR. GARTNER: In your
6 proposal or your vision, is that berm
7 grass covered? Is that a bed, you know?

8

9 MR. TISCORNIA: No, we think it
10 would be -- as I said in my comment, I
11 think we believe it would be vegetated
12 with the plants that are in the land
13 ordinance of the Town of Hilton Head.
14 I think there's Crepe Myrtles or Wax
15 Myrtles, I'm not sure what it is, I've
16 been in the ordinance and looked at
17 that, there's a whole list of plants
18 that you view as being acceptable and I
19 think those would be acceptable planted
20 on top of that berm.

21

22 MS. WELCH: But
23 wouldn't that make it higher than it's
24 allowed to be?

25

26

1 MR. TISCORNIA: Oh no, it would be
2 well within the 34 to 1 glide slope and
3 the trimming as those trees or bushes
4 grew because the ordinance, the land
5 ordinance that you have signed clearly
6 says that you have to maintain that
7 glide slope beneath the 34 to 1 ratio;
8 so a few foot berm at a thousand feet
9 down the runway it should not fall
10 within that 34 to 1. But again, I'm
11 not a civil engineer, I don't have all
12 the, you know, drawings and the
13 documents in front of me at this point
14 in time.

15
16 CHAIRMAN THEODORE: All right. Any other
17 questions for this gentleman? Thank you,
18 sir.

19
20 MR. TISCORNIA: You're welcome.

21
22 CHAIRMAN THEODORE: All right. Sir, would
23 you like to come up? Please sign your
24 name and please state your full name

25
26

1 and again let me just remind you try to
2 keep the time to about four minutes
3 would be great, thank you.

4

5 MR. FISHER: Mr. Chairman,

6 Members of the Board, my name is Jim

7 Fisher. I'm a resident of Port Royal

8 Plantation. I'm extremely disappointed

9 in seeing the plan. We all knew the

10 trees were going to be cut but here

11 comes the plan. I'm not only just

12 disappointed, I'm shocked.

13 I'm here to ask you to take this

14 proposal, reject it, send it back to

15 the drawing board because what you're

16 -- is being asked is not what was

17 promised.

18 These -- if this sails through

19 this board it sets a dangerous

20 precedent for all the other areas

21 around that airport, we're talking Port

22 Royal Plantation, we're on the other

23 side of the runway we know our cutting

24 is coming.

25

26

1 I'd like to bring a couple facts
2 out that this proposal is completely
3 overwhelming but I'd like to give you a
4 couple facts.

5 You will find this chart in your
6 package. I'm sure you can get down to
7 the smallest print in the world --I
8 lost that somehow--
9 That little tiny
10 chart that's buried in there in extra
11 small print, that little chart in the
12 bottom left of your screen there that
13 says that you cut 12,940 trees. I
14 don't know if that's been brought out.
15 I mean that could hit on the Packet
16 headlines with facts exposed, 12,940
17 trees cut. Okay, if that's required by
18 the FAA so be it. How many are left?
19 Look at that chart right there; it says
20 1,666 trees -- 1,766 trees left, that's all.

21 What's the effect? Well you can
22 go to the next -- the chart in your
23 package. This chart shows what's left
24 and you will show that's a dense buffer;
25 Town Council said it, everybody said
26 it, we're going to have a dense buffer.

1 I'd like you to take a real
2 careful look at that chart, there are
3 little circles there, those are in --
4 my red writing existing, those are
5 existing trees. So what you have is
6 existing trees, 1,766 stalks, that's
7 what you got left. I mean I'll call it
8 a stalk somebody else is going to call
9 it a tree because that's not what's
10 left. If you cut -- if those are
11 plantings that are there -- that's not a
12 dense mitigation, that's leaving all of
13 those little circles, every one of them
14 circles are what you see, those are
15 what you saw before.

16 When we plant it we can talk about
17 thousands of dollars, we're talking
18 about probably a \$75 million project
19 and we're going to worry about the
20 money? But what we've got to look at
21 is what is the cost to do the job right
22 for the Town and the FAA.

23 We can also look at some
24 statistics of how long these will live,
25 so we plant these new trees, 1,294
26 trees are being planted, we cut down

1 12,940, we only got 1,294 and probably
2 50 percent of those will die. What can
3 you do? I mean first thing, right up
4 front, I think you've got to replace
5 stalks with real bushes, trees or
6 something. If there's 1,766 trees left
7 maybe we need to add 1,766 more bushes
8 to make the dense buffer that this town
9 asked for and we were promised as
10 residents.

11 I think this council should very
12 carefully attend -- every person here
13 should be on the 4-3 -- April 3rd you
14 should be at that Environmental
15 Assessment Review. You better know
16 what's coming because that's when the
17 next -- that's the runway extension.
18 We don't want to be behind the
19 Eight ball like we are today.

20 In closing there's -- the FAA has
21 got us in a rock and a hard place, got
22 the Town there, it's got the County but
23 bottom line is it the big government
24 FAA telling us what to do? Absolutely.

25 Now we're getting into legal
26 challenges of threatening things. How

1 many trees we cut down we just reduce
2 the bird mitigation by probably
3 95 percent because we cut down 12,400
4 trees and we're worried about some
5 grass that maybe bring birds? Give me
6 a break. I mean we could call this
7 bait and switch. There's some comments
8 the FAA Director said, 'We are Hilton
9 Head. We are not south Georgia. We
10 don't go and destroy your environment.
11 We want to be a good neighbor. We want
12 to be politically correct and do what's
13 right.'

14 All of those comments, bait and
15 switch, con job, whatever it is, it's
16 not the County, it's not the Town, not
17 this board. The FAA is dictating our
18 life and that's what you want? We got
19 a real problem. Thank you very much
20 for listening.

21
22 CHAIRMAN THEODORE: Before you go I'm
23 trying to decipher out of your comments
24 there, we are where we are.

25

26

1 MR. FISHER: We are where we are.

2

3 CHAIRMAN THEODORE: What is -- in your
4 comments there, your recommendation
5 would be to plant more vegetation?

6

7 MR. FISCHER: Correct, sir, I
8 agree. If you look at those circles,
9 look at the dark circles, those are --
10 represent bushes that are going -- not
11 trees but bushes, you could -- they
12 call it you cut trees and they're
13 planting bushes, that's the -- another
14 definition but if you look at the dark
15 circles on that chart those are what
16 are coming back.

17 All of those little round
18 circles, the light circles, those are
19 existing, "trees". Those are those
20 little stalks you saw. There may be
21 some big ones that's why I said
22 probably of the 1,700 trees left
23 there's probably a couple hundred, at
24 least 200 real trees left. Does that
25 answer your question?

26

1 But basically I'm asking that you
2 need to do a lot more. It needs to be
3 the dense buffer that was promised.
4 It's going to be expensive and I
5 applaud you for bringing forth the
6 issue on irrigation.

7 If I built a house and I planted
8 my bushes in my -- around my yard and I
9 watered them once a month I'd be
10 planting everything from scratch. You
11 need to water those bushes, some of
12 them in the summer, weekly, you can't
13 get away with it, they're going to die
14 and then we're going to say, oh I'm
15 sorry.

16
17 CHAIRMAN THEODORE: All right. Does
18 anybody have any questions for this
19 gentleman?

20
21 MR. GARTNER: I guess my
22 only question is, what number then
23 becomes amenable to you and your
24 plantation and in the surrounding
25 residential neighborhoods just so that
26 we can start getting this to move

1 forward? You know, maybe we could
2 set up a situation where we're demanding
3 those numbers or asking for those numbers
4 as a prerequisite to passing this and
5 getting things moving on it.

6
7 CHAIRMAN THEODORE: You know that one of
8 the things that the town has said is
9 that they're going to go out there and
10 plant this and then they're going to be
11 a field supervision and then if there's
12 any holes in that, visual holes then
13 those will be identified and that will
14 be something that -

15
16 MR. GARTNER: Correct.
17 Which, in my opinion is something that
18 I would be comfortable passing,
19 however, it sounds like, sir, you
20 wouldn't be, so I'm wondering is there
21 a number that makes sense to you?

22
23 MR. FISHER: I think there's a
24 real good answer to your question;
25 there is no good number. What you
26 really want to do is drive down Beach

1 City Road and not see what you got out
2 there. That was what was promised, a
3 buffer.

4 And the answer is, if you look at
5 all those dark circles do you see holes
6 in those dark circles? Every dark
7 circle needs to be filled in with
8 something that is a vegetation so you
9 don't see it and I will agree that with
10 the previous comments regarding noise,
11 ground noise and smell I mean I live on
12 the other side of the runway, I get the
13 jet smell. I was on the golf course
14 last week and guess what, it was
15 overwhelming.

16
17 CHAIRMAN THEODORE: But we can't -- what
18 we're here is to address this
19 application.

20
21 MR. FISHER: Trees help. And more
22 trees help than less trees.

23
24 CHAIRMAN THEODORE: All right.

25
26 MR. FISHER: That's basically the

1 answer and the more that you can fill
2 in those holes the better off you're
3 going to be.

4

5 CHAIRMAN THEODORE: All right, thank you
6 sir.

7

8 MR. FISHER: Does that answer
9 your question?

10

11 MR. GARTNER: Kind of; yeah.

12

13 CHAIRMAN THEODORE: Thank you, sir.

14

15 MR. FISHER: Thank you very much
16 for allowing me to speak.

17

18 CHAIRMAN THEODORE: Is there any other
19 citizens that would like to speak?

20 Sir, please come up.

21

22 MR. SODEMANN: I get a
23 feel just from the list of plants that
24 we've got here I mean there's a variety
25 of heights that these things grow to
26 obviously the Red Cedar being the

1 tallest but the Wax Myrtles are
2 relatively low to the ground, correct?
3 Or they can grow tall, I know, if you
4 don't prune them down.

5
6 MR. ROAN: I was
7 actually waiting on an e-mail so I keep
8 checking my phone from a commercial nursery in
9 Hardeeville, they're going to send me
10 examples of pictures. They're all
11 spec to be ten feet tall at
12 installation. I had an understanding
13 it was nine feet.

14
15 CHAIRMAN THEODORE: Okay.

16
17 MR. ROAN: They're
18 eight-feet on center as represented
19 here. You know, it's not just the dark
20 circles, every one of these is a plan,
21 I mean I'm not trying to -

22
23 CHAIRMAN THEODORE: I would say, for the
24 most part, these materials are the
25 heartiest naturalized plant material
26 that you could probably use that would

1 be -- what they're trying to do is
2 control their height, you know, that
3 whatever goes in there they want it to
4 be maintainable within a certain height
5 zone and of these selections, you know,
6 these are probably the heartiest that
7 you can have plus like a Wax Myrtle, I
8 mean you've seen those, those are
9 pretty dense vegetation, as well, so
10 from a selection of material they
11 appear to be, you know, other than
12 actually planting trees that would grow
13 higher these would be good fillers to
14 grow in and all of them are evergreen,
15 as well, so, you know, it's a perennial
16 density of plant material.

17
18 VICE CHAIRMAN SODEMANN: The Yaupon
19 Holly and the Wax Myrtle are pretty
20 dense materials and they're not
21 terribly tall versus the Red Cedar and
22 the Little Gem Magnolia tall.

23
24 CHAIRMAN THEODORE: Correct.

25
26

1 VICE CHAIRMAN PARKER: So we've
2 got a variation in height.

3

4 MR. GARTNER: The Yaupon
5 Holly it's more hedge like, I mean you
6 could almost -

7

8 CHAIRMAN THEODORE: Well yes,
9 it can be. The Yaupon Holly is what
10 you see in a lot of forests; it has
11 kind of oak leaf type vegetation that
12 can grow as an understory where it
13 grows along the beach line, that kind
14 of stuff; it's a very hearty evergreen
15 material that can get very dense, as
16 well, so and probably the most native
17 to Hilton Head is the Yaupon Holly.

18

19 CHAIRMAN THEODORE: Did you fill in your
20 name, sir?

21

22 MR. CLEYART: Yes, I did.

23

24 CHAIRMAN THEODORE: Please state your name
25 for the record.

26

1 MR. CLEYART: My name is Dan Cleyart and
2 I live at 8 Gun Powder Place on Hilton
3 Head Island.

4 Mr. Chairman, Honored Members of
5 the Board, I just want to make a couple
6 of short comments basically related to
7 why we moved to Hilton Head Island and
8 as you know the Mayor's Vision Task
9 Force which was I think about a year,
10 two years ago, identified as their
11 first vision is to protect the natural
12 beauty environmental resources and
13 unique sense of place in our sea island
14 and I know you guys do all you can to
15 fill that vision but unfortunately
16 sometimes I think we're swaying away
17 from that vision.

18 You know, the lure of Hilton Head
19 Island is directly tied to this vision
20 and the environment of Hilton Head. My
21 wife and I looked at many, many
22 communities along the coast, North
23 Carolina, Georgia, Florida, and once we
24 discovered Hilton Head Island we didn't
25 want to go anywhere else, so because of
26 that, of its environment and its

1 natural beauty, and we need to make
2 sure we preserve that and I fear that
3 the island is moving away from that
4 vision as indicated by what's happening
5 here at the airport and I think we need
6 to be a lot more sensitive before
7 taking any action that's going to be
8 contrary to this vision.

9 As you know, whatever's done today
10 concerning this plan is probably going
11 to set the precedent for the future and
12 as you know they're going to be cutting
13 more trees on the north end for the
14 airport expansion all the way across
15 toward Palmetto Hall, all of those
16 trees are going to be probably
17 butchered. The same thing on the south
18 end. God forbid what's going to happen
19 to Pineland Station, which is already
20 hurting, when they cut all those trees
21 behind it and all the trees around the
22 Wells Fargo Bank and that whole area of
23 278, that whole corridor is going to
24 change when all those trees go and
25 that's my concern; we're changing the
26 environment of this island and

1 unfortunately a lot of folks on the
2 island, citizens, permanent citizens
3 don't realize what's going on,
4 especially folks who may live towards
5 the south of the island, oh that's the north
6 side we don't care about the north side but
7 they should care about the north side. I think
8 it's very important that you folks
9 review this plan very carefully and
10 perhaps based on a lot of comments,
11 today, is wait before we make a
12 decision, look at other alternatives,
13 make sure the right plan is going to be
14 in place for mitigation. If a berm
15 needs to be done I think you really
16 need to seriously look at that, so
17 that's all the comments I had and I
18 thank you very much for your time.

19
20 CHAIRMAN THEODORE: If you don't mind if I
21 ask, you know, we're focused on this
22 application. The clearings -

23
24 MR. CLEYART: Yes.

25
26 CHAIRMAN THEODORE: -- have already been

1 done so we're to address, you know,
2 what they're proposing for the
3 mitigation and obviously we're
4 responding from the aesthetic side of
5 how that's going to look and obviously
6 we have parameters that we have to work
7 within because obviously all this was
8 done for the airport and -

9

10 MR. CLEYART: Correct.

11

12 CHAIRMAN THEODORE: -- and with safety
13 measures that drove this. What are
14 your recommendations? You know, that
15 we are where we are here and if this is
16 to set the precedence for across the
17 street, the other side, that sort of
18 thing, in your mind what do you see as
19 something that would be in keeping with
20 Hilton Head?

21

22 MR. CLEYART: First of all, if I can
23 elaborate a little bit. I think the
24 expansion of the airport is a total
25 waste of tax money.

26

1 CHAIRMAN THEODORE: Okay. Now we know all
2 that.

3

4 MR. CLEYART: You should understand
5 that, first, but we're talking about
6 spending another \$500,000 in mitigation
7 of trees which we wouldn't have had to
8 do -- which is only the beginning
9 because you're going to cut more trees.

10

11 CHAIRMAN THEODORE: I know.

12

13 MR. CLEYART: I understand what your
14 question is and my response to that is
15 I'm not a landscaper. I just don't
16 want this island to continue in a
17 direction that's contrary to what the
18 philosophy of this island was way back
19 under Fraser.

20

21 CHAIRMAN THEODORE: And I understand all
22 that but that's outside of our realm of
23 focus right now and what I want to do is
24 be able to address your concerns with
25 where we are right now knowing what the
26 parameters are, you know, that you can't

1 plant another tree that's grow up that
2 will end up being pruned is what do you
3 see that we need to pay attention to
4 that would make sure that the solution
5 in this mitigation represents what
6 Hilton Head is? Now I'll have my
7 opinion but I wanted to get your
8 opinion of what would this -- what do
9 you feel this needs to be versus what
10 the County and the airport have
11 proposed so far?

12

13 MR. CLEYART: I think you heard from
14 the two previous gentleman what they
15 proposed and that's increasing the
16 density of the plantings or building a
17 berm and I fully support that.

18

19 CHAIRMAN THEODORE: And is there a
20 measurable density of plant material
21 that -

22

23 MR. CLEYART: Again, I'm not a
24 landscaper. I don't want to be able to
25 see through the trees, number one.

26

1 CHAIRMAN THEODORE: So that's the main
2 thing, you don't want to be able to
3 have a visual penetration through that.

4
5 MR. CLEYART: And part of that visual
6 penetration, again, is perhaps noise
7 mitigation for the folks that live in
8 Palmetto Hall and in Mitchelville, I
9 think that's very important.

10
11 CHAIRMAN THEODORE: Okay. Great. Thank
12 you.

13
14 MR. CLEYART: Thanks for the time.

15
16 CHAIRMAN THEODORE: All right. Any other
17 citizens? Ma'am, would you like to
18 come up, please.

19
20 MS. CLEYART: Hi, everybody. My name
21 is Karen Cleyart. I live in Port Royal
22 Plantation and I just have a few
23 comments.

24
25 CHAIRMAN THEODORE: Now do you -- but let
26 me make sure, do you have new comments

1 in addition to what has already been
2 stated?

3

4 MS. CLEYART: Yes, I do.

5

6 CHAIRMAN THEODORE: Okay. Great. Thank
7 you.

8

9 MS. CLEYART: All right. First I
10 would like to, with all due respect to
11 Mr. Kubic, question his use of the word
12 'suggest' 'assume' because the FAA has
13 rules and guidelines and somebody needs
14 to be looking at those and finding exactly
15 what can be put in place and I don't
16 want to use the word 'trust' though
17 he did, I wouldn't be trusting what the
18 airport says is acceptable, I would
19 want to know myself. I don't have the
20 FAA rules in front of me but I'm sure
21 someone on this board would be able to
22 find them.

23 I'm also questioning the trimming.

24 It's already been done. It's a done

25 deal. You know, do it now ask

26 forgiveness later. But in all my time

1 I've had tree trimming done nobody has
2 trimmed 80 percent more than I asked
3 them to do and the trees are supposed
4 to be trimmed at 30 feet and 50 feet
5 and they're down to six to seven feet,
6 that's 80 percent shorter. 12,000
7 trees are gone, you know, it's a done
8 deal. As they said before, we need a berm.
9 Don't let the airport people tell you
10 that it can't be done. The only reason
11 it couldn't be done is if they slam
12 through 5,400 feet. If they stay at
13 the 5,000' of the 4,700 there's room for
14 a berm so I'm a little upset about
15 that.

16 And again, I'm concerned about
17 what's going to happen to the
18 vegetation at Pineland Station when
19 they continue off airport property.
20 They weren't very careful on airport
21 property, what the hell are they going
22 to do once they get off? Is it going
23 to be like Mayor Daley taking care of
24 -- airport in the middle of the night,
25 is that what's going to happen again?
26 That's my concern.

1 Worrying about the birds, I'm
2 sitting here laughing. If there aren't
3 any trees where the hell are the birds
4 going to live? They aren't going to
5 live in the grass, most of them are
6 looking up into the sky, they want to
7 hang out in the trees. Sorry for my
8 profanity, I'm a little concerned here
9 but, you know, he's suggesting only
10 Bermuda grass. I'm sure in the FAA
11 requirements there are a list of other
12 things that could be planted in place
13 of that. Thank you.

14
15 CHAIRMAN THEODORE: Thank you.

16
17 MS. CLEYART: Questions? No.

18
19 CHAIRMAN THEODORE: Thank you. Sir, would
20 you like to come up, please.

21
22 MR. KEEVER: My name is Fred
23 Keever. I live in Palmetto Hall and
24 I'd like to say, first of all, that the
25 airport being there and the effects of
26 it don't concern me particularly where

1 I live so I'm not here because of my
2 own concerns about that particular -- I
3 am concerned about the fact that we're
4 moving in a direction that scares me
5 for the future in terms of some
6 people's saying what we're heading
7 towards compared to where we've been.
8 I've lived here ten years, came here
9 for 20 years before. This is a
10 paradise. Always considered it that,
11 and I want to keep that.

12 I think we need to think about how
13 we can all work together to fix the
14 catastrophe we have right now and when
15 we talk about trust I don't know how we
16 managed to cut all those trees when,
17 from my understanding, nothing close to
18 that was going to happen so I can't
19 trust anybody who does something that
20 ridiculous compared to what I thought
21 the plan was but that's only my own
22 feeling. I have a general growing
23 distrust of government and the
24 bigger the government the more distrust I have.
25 Local government is the
26 answer to a lot of things now I want to

1 get to one point -

2

3 CHAIRMAN THEODORE: But some of this is a
4 forum elsewhere than where we are
5 here, we're the aesthetics to look at
6 the application, so we're focused on,
7 you know, how do we move forward, you
8 know, based on the proposal -- is to
9 analyze that and determine if that's
10 the, you know, the aesthetic approach
11 that is acceptable to this board.

12

13 MR. KEEVER: Yeah, I understand
14 and I'm going to get to that. I just
15 had to voice a little bit of my
16 concern. I've always been proud of
17 this place, proud of the way we do
18 things and have done things in the past
19 and I'm hopeful you guys will keep an
20 eye on everything and try to get this
21 fixed as best we can.

22 The one thing I'd like to suggest
23 is this: I can't tell and I don't know
24 if anybody else can, exactly what the effect
25 of what's been proposed is. I know
26 Bermuda grass is not going to do

1 anything to solve the visual beauty
2 problem that we have missing now, so
3 obviously a lot of vegetation needs to
4 be put in there and what I would
5 suggest is this; that we have a
6 representative from each plantation
7 that's affected by this, perhaps even
8 some people from the commercial side,
9 keep it limited, maybe not the
10 commercial side, just Palmetto Hall and
11 Port Royal. Have a person from the
12 airport committee be involved with the
13 final determination of what's going to
14 be done - might have to be in two steps.
15 Maybe the County does what we kind of
16 think is okay but then we go back and
17 look at it and say, it really doesn't
18 do what we need to do in these areas or
19 whatever. So that we can get it so it
20 does solve the problem from both a
21 visual standpoint and I'm really more
22 concerned with that part of it, but
23 also the noise mitigation and
24 vegetation certainly helps that and
25 also brings back that feeling of
26 privacy and non-commercial feel that

1 Hilton Head has always represented to
2 everybody. And the berm situation, if you
3 drive out 278 and look at Sun City, for
4 example, that's a perfect example of a
5 place that is from the road much more
6 beautiful with the berms but also it
7 provides real protection of those
8 communities from the noise of 278 so I
9 think the berm idea is great. I think
10 we could do without it maybe if we got
11 enough of the right plantings and I
12 think a key would be to sit down with
13 somebody that knows about the kind of
14 plants you're talking about, look at the
15 native plants that would grow well
16 here. You've got the proper irrigation
17 and make sure that we'll have enough
18 but that won't grow so high that we
19 have this problem in the future. I
20 really feel we've let this problem go
21 much longer than we probably should
22 have but, you know, getting it solved
23 now is the issue from where we are. So
24 if we could have some work together in
25 that regard, and I see you as the people
26 who will have or need to have the final

1 say so hopefully of what is done
2 because you represent the people here.
3 The County does to a degree, too, but
4 the FAA doesn't seem to care much about
5 anything except spending their money
6 and getting it done and getting it over
7 with and I do want to see that we have
8 some people watching them and trying to
9 solve the problem as best we can. I
10 appreciate your taking the time to listen
11 to me, and if you'll consider that idea I
12 appreciate that, also. Thank you. Any
13 questions?

14
15 CHAIRMAN THEODORE: Thank you. Any
16 questions?

17
18 MR. GARTNER: I don't
19 have any.

20
21 MR. KEEVER: Okay thanks.

22
23 CHAIRMAN THEODORE: Thank you. All right.

24
25 MR. ROAN: I have a point of edification from Mr.
26 Williams I've been asked to clarify.

1 If it was 13,000 caliper inches that
2 were removed, the total number of
3 inches of trees collectively taken out there are
4 actually only 1,300 or I'm
5 sorry 1,130 trees taken out totaling
6 1,300 caliper inches.

7
8 CHAIRMAN THEODORE: So almost the
9 number of trees that are taken out are
10 the trees that are to be put back in?

11
12 MR. ROAN: Yeah, yeah.

13
14 CHAIRMAN THEODORE: So close.

15
16 MR. ROAN: In inches.

17
18 MR. CHESTER WILLIAMS: This chart,
19 though, showed the replacement trees
20 are caliber inch of trees, also, it's
21 not number of trees.

22
23 MR. ROAN: Yeah,
24 correct -- trees taken out just so --
25 just to keep the fact based is 1,133 -
26

1 MR. WILLIAMS: I think the
2 statements were that the trees were
3 two-inch trees, so if you've got 1,700
4 caliper inches you've got half of that
5 number of trees that are going in.

6
7 CHAIRMAN THEODORE: Excuse me? Mike -- what
8 is proposed, though, is -- meets the
9 mitigation criteria for the trees that
10 were removed?

11
12 MR. ROAN: Yeah. The
13 mitigation criteria was one replacement
14 tree for every ten caliper inches that
15 were removed, hence I think it was
16 1,300 plus our minus caliper inches
17 that were taken out. There were 1,300
18 -- 1,294 trees being replaced, those
19 caliper inches were in some total of
20 1,130 trees that were taken out.

21
22 CHAIRMAN THEODORE: Okay. And that's
23 according to the LMO?

24
25 MR. ROAN: That's
26 according to the LMO, correct.

1 CHAIRMAN THEODORE: And that's including
2 the trees that were taken out of the
3 core area and they're being planted
4 over in the buffer zone?

5
6 MR. ROAN: Correct.

7
8 CHAIRMAN THEODORE: Okay.

9
10 MR. WILLIAMS: Good afternoon
11 ladies and gentlemen. My name is
12 Chester Williams. I'm a local attorney
13 here on Hilton Head Island.

14 Mr. Chairman, ladies and
15 gentlemen, I appreciate the time here
16 before you. I am here, at this point,
17 on behalf of St. James Baptist Church.

18 We're here today because of the
19 amendments to the Land Management
20 Ordinance that the Town Council
21 approved in May of 2010 that set a
22 scheme for the trimming and removal of
23 trees on Hilton Head Airport that only
24 the airport can take advantage of,
25 nobody else can take advantage of these
26 rules.

1 A substantial portion of the
2 on-airport property has -- it's been
3 scraped clean essentially and now we're
4 here to review the County's mitigation
5 plan which is required by the
6 amendments to the code that were passed
7 in May of 2010.

8 LMO Section 16-4-403-C.b.small roman
9 i small roman iii, which is part of the
10 new amendment there requires that the
11 airport adjacent use buffers and
12 adjacent street buffers shall be a
13 minimum of 75 feet in width -- a lot
14 easier for me to point -- and this is
15 -- this is the 75-foot adjacent street
16 buffer along Beach City Road. This is
17 the 75-foot adjacent use buffer along St.
18 James Baptist Church and that shifts over
19 here also to this area and then there's
20 an adjacent street buffer along Dillon
21 Road. The Code requires that it be a
22 minimum of 75 feet.

23 Note in particular the specific use
24 of the terms adjacent use buffers and
25 adjacent street buffers and then open
26 up your LMO, go take a look at your

1 Article 8 Chapter 5, which sets out the
2 standards for buffers, Section 16-5-801.A
3 provides that the function of buffer
4 areas is to provide aesthetically
5 acceptable, visual and spatial
6 separation between adjacent land uses
7 and Section 16-5-801.B provides that the purpose
8 of buffer areas is to enable the
9 juxtaposition of land uses of different
10 types thereby accommodating the
11 developer, adjacent landowners, and the
12 public's interest in a visually
13 attractive environment. I don't think
14 any of us, right now, would say that
15 what's out there right now is a
16 visually attractive environment.

17 And it goes on to say, also, to
18 minimize any negative effects that land use will
19 impose on its neighbors the buffers
20 shall be provided between uses and
21 adjacent to public streets.

22 Go a little bit further in the
23 Code on the section on buffers and take
24 a look at permitted activities in
25 buffer areas, under Section 16-5-808,
26 which sets out the permitted activities

1 and street buffers and under Section
2 16-5-809, which says the permitted
3 activities in other buffer areas which
4 includes adjacent use buffers.

5 Subsection B in each of those
6 provides that sewer service lines are
7 allowed in buffers provided they are
8 approximately perpendicular to the
9 street right-of-way for the common
10 property line, that is they cross from
11 the street to the property through the
12 buffer or from one parcel to the next
13 parcel perpendicular through the
14 buffer, that's a permitted use of the
15 buffer area.

16 Those sections go on to say,
17 though, if installed approximately
18 parallel to the street right-of-way or
19 the common property line, and let me
20 digress a bit and that's the situation we
21 have here, a sanitary sewer force main
22 that runs parallel through this buffer
23 along the right-of-way of Beach City
24 Road and it continues on, it turns here
25 and it runs parallel along the boundary
26 line of St. James Baptist Church

1 through this adjacent use buffer.

2 If installed approximately
3 parallel an equal amount of buffer
4 shall be required to substitute for the
5 area of vegetation removal. If the
6 easement is parallel along the required
7 buffer it may -- it may be included
8 within the required buffer as long as
9 the easement will still function as a
10 buffer.

11 Look at the comments on the plan
12 here. Existing sanitary sewer force
13 main, approximate location. No new
14 plantings within the 15-foot easement.
15 You don't have a 75-foot buffer here,
16 you have a 60-foot buffer here because
17 you've got 15 feet in there where you
18 can't plant anything.

19 The Code says in that situation in
20 an adjacent use buffer or an adjacent
21 street buffer that an equal amount of
22 buffer shall be required to substitute
23 for the area of vegetation removal.

24 This line, in order to comply with
25 the code, needs to come back another
26 15 feet and over here it needs to come

1 back between I'd say eight and ten feet
2 otherwise the plan does not comply with
3 the LMO requirements for buffers.

4 Take a look at what's proposed
5 here, also. The -- I don't know
6 herringbone or whatever this sort of
7 pattern is here, area to be grassed,
8 Bermuda, it says. Area to be grassed,
9 alternate methods.

10 The amendments to the Code that passed
11 in May of 2010 that allow for all this
12 saying that this plan shall include the
13 planting of low -- shall include the
14 planting of low growing native plants
15 on non-wetland non-buffered portions of
16 on airport property. Non-wetland,
17 non-buffered, that includes this area,
18 the crosshatched area and again whatever
19 you call that.

20 These plants help to maintain the
21 storm water quality but requires native
22 plants, I don't think Bermuda's a
23 native plant.

24 And then it goes on to say,
25 examples of plants that could be used
26 in these -- are Seaside Juniper, native

1 grasses or Wax Myrtles, Saw Palms,
2 Needle Palms and some species of native
3 blueberries. I don't think Bermuda is
4 included in there. I mean this plan
5 does not accurately reflect the
6 requirements of the Land Management
7 Ordinance and it should not be approved
8 as is but think about it, you know,
9 these sections of the Code that were
10 amended in May of 2010 were the result
11 of many, many months of back and forth
12 negotiation among the Town staff and
13 locality and the Town Council and
14 everybody knew what this stuff said.
15 You know now the County's saying, oh,
16 we don't want to plant native -- you
17 know, we don't want to plant native
18 grass there we want to plant something
19 else. Well, that horse left the barn
20 already. If they want to come back and
21 ask the Town to change the code again
22 then let them do that but right now
23 this plan that's before you does not
24 comply with the requirements of the
25 Land Management Ordinance. That

1 15-foot wide easement along Beach City
2 Road is 20 percent of that buffer area.
3 One-fifth of it you can't plant
4 anything in. It effectively reduces
5 that buffer to only 60 feet and it
6 effectively reduces the buffer along
7 St. James Baptist Church to as little
8 as 65 feet. That was not the intent of
9 these provisions of the code when they
10 were enacted in May of 2010.

11 Considering the fact that a
12 substantial portion of the airport
13 property has been scraped clean and
14 that the County is now before you with
15 a mitigation plan that does not meet
16 the requirements of the Code. I think
17 y'all need to reject this application,
18 deny it, send the County back, have
19 them come back to you with a plan that
20 does meet the requirements of the Code.

21 Now with that, let me stand down
22 from speaking on behalf of St. James
23 and let me speak on behalf of myself,
24 personally.

1 I am appalled at the statements of
2 the County Administrator today. We
3 want to follow the law he says but in
4 some cases we don't want to follow the
5 law, we want to plant something else,
6 the FAA wants us to plant something
7 else. And then he goes on to say that
8 if there is an event and somebody's hurt that
9 he expects the Town is going to be
10 responsible for helping pay for those
11 damages because the Town -- if the Town
12 insists that the County follow the Code
13 -- I don't see where the County
14 Administrator gets off making those
15 sorts of comments. Thank you.

16
17 CHAIRMAN THEODORE: If I may ask you a
18 question. When you said that they've
19 come in and scraped the land, I mean
20 obviously there's been some serious
21 vegetation removal. I'm sure you -

22
23 MR. WILLIAMS: It's been scraped
24 clean. It's been cleared -- it's been
25 cut down to the ground, yes.

26

1 CHAIRMAN THEODORE: In your opinion, and
2 you obviously probably sat in a lot of
3 those meetings and stuff, is where we
4 are today based on all the
5 discussions that had been agreed upon
6 that this is what the direction -- you
7 know, that everybody approved and led
8 to and we're just at a point of what is
9 the mitigation from this point forward
10 and whether it's the, you know, the
11 75-foot buffer with the utility
12 easement through there or it needs to
13 be wider but where we are today is
14 this -

15
16 MR. WILLIAMS: No, Mr. Chairman,
17 what's done is done. You know, St.
18 James Baptist Church filed a lawsuit
19 against the Town and the County when
20 the permits were approved to allow for
21 that clear cutting. Once that got to
22 the South Carolina Court of Appeals and
23 the automatic stay that is normally in place
24 in an appeal like that was lifted -

25

26

1 CHAIRMAN THEODORE: Yeah.

2

3 MR. WILLIAMS: -- and those trees
4 were cut that case became moot.

5

6 CHAIRMAN THEODORE: Okay. I was just
7 trying to understand when you said they
8 came in and scraped it -

9

10 MR. WILLIAMS: From my personal -- I mean
11 when I say scraped it that area was -- the
12 area there that's shown is a cross hatched in
13 the herringbone pattern that has been
14 completely clear cut down to the ground.
15 I mean it looks like a devastated area
16 and, yes, that's -- from a personal
17 standpoint, yes, that's what I fully
18 expected to see when this was all over
19 and done with all those trees just
20 absolutely gone.

21

22 CHAIRMAN THEODORE: But all that was
23 based on the parameters that FAA and whatever
24 outlined.

25

26 MR. WILLIAMS: Absolutely not.

1 No, the FAA did not require that that
2 area be clear cut. Correct. Did not.
3 Not, at all. The FAA requires that it
4 maintain a 1 to 34 slope and look at this,
5 you see this line right here, it says 40.
6 You see this line right here that says 50? At
7 this point from the edge of the runway
8 to meet the 1 to 34 slope you have to
9 be no more than 40 feet, at this point
10 no more than 50 feet, didn't have to
11 clearcut that area in order to comply
12 with the FAA requirements. The County
13 did that because of a monetary
14 reasoning and only a monetary reason
15 because once it's clear cut then they
16 don't have to worry about later on
17 going back and trimming trees again to
18 maintain that 1 to 34 slope.

19
20 CHAIRMAN THEODORE: Okay.

21
22 MR. WILLIAMS: And now they're
23 here with a mitigation plan that does
24 not meet the Code requirements and they
25 ought to be sent packing back home to
26 come up with a plan that does meet the

1 mitigation requirements and then open
2 the discussion as to the aesthetics of it.

3

4 CHAIRMAN THEODORE: All right. Thank you.

5 Is there any questions for Mr.

6 Williams?

7

8 VICE CHAIRMAN SODEMANN: Explain to
9 me again how -- how having the sewer
10 easement within the buffer does not
11 comply with the LMO even though it got
12 buffering on or tree plantings and
13 vegetation planting on both sides of
14 that?

15

16 MR. WILLIAMS: LMO Section
17 16-5-808 that deals with permitted
18 activities and buffer areas. I'm
19 sorry, that's permitted activities and
20 street buffers and LMO Section 16-5-809
21 says that if a sewer line like that is
22 installed generally perpendicular
23 through a buffered area along the
24 roadway right-of-way then an equal
25 amount of buffer shall be required to
26 substitute. You got a 75-foot wide

1 buffer, you can't use 15 feet of it so
2 the Code says you've got to add another
3 15 feet to it to make up from the area
4 that would otherwise be in the buffer
5 that you can't use so if this buffer
6 line here in order to comply with the
7 Code instead of being 75 feet, keep in
8 mind the Code says that's a minimum of
9 75 feet, it doesn't say it's a maximum
10 of 75 feet, it says minimum of 75 feet.
11 This line ought to be back another 15
12 feet for 90 feet of the roadway
13 right-of-way, those are the Code
14 requirements.

15

16 VICE CHAIRMAN SODEMANN: Doesn't that also
17 refer to just the density of what's
18 replanted in there -

19

20 MR. WILLIAMS: Not at all.

21

22 VICE CHAIRMAN SODEMANN: -- in turn?

23

24 MR. WILLIAMS: Not at all. It's
25 solely the linear distance of the
26 buffer.

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CHAIRMAN THEODORE: But you were stating something in there where you can supplement the density of that buffer.

MR. WILLIAMS: If an easement is parallel within the required buffer it may be included within the required buffer as long as the easement will still function as a buffer. That easement, if it's clear cut, doesn't function as a buffer, clear, it's plain and simple.

CHAIRMAN THEODORE: So if you went to the 75-foot that would -

MR. WILLIAMS: 75 plus 15 to Make up for the -

CHAIRMAN THEODORE: If you had the full 75, in your mind, that would be a better solution to meeting that requirement?

MR. WILLIAMS: If you could plant

1 over that 15-foot easement so that you
2 had a full depth of 75 feet of
3 plantings but you only have a depth of
4 60 feet of plantings.

5
6 CHAIRMAN THEODORE: Right.

7
8 MR. WILLIAMS: You know, and
9 that's simply not what the Code allows
10 for.

11
12 VICE CHAIRMAN SODEMANN: All right.
13 What about if the density was increased
14 of with what's there to increase that
15 visual -

16
17 MR. WILLIAMS: These provisions of
18 the code deal exclusively with the
19 linear the two-dimensional linear
20 distance to the depth of the buffered
21 area.

22
23 CHAIRMAN THEODORE: Well, I interpret that
24 one piece a little different there it
25 says -

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MR. WILLIAMS: Yeah.

VICE CHAIRMAN SODEMANN: -- if it's functioning

CHAIRMAN THEODORE -- if it still functions
as a buffer within that width that included
the easement through there. Okay. Any
other questions? Thank you.

MR. WILLIAMS: Thank you.

CHAIRMAN THEODORE: Any other citizens?
Well, we'll get to you in a minute. Now
again, I'd like to repeat, you know,
that let's please not duplicate the
same comments, that we've gotten a good
amount of feedback so please introduce
if you have a new comment. All right.

MR. DON SCHWARTZ: I'd just make an
observation and then a recommendation.
First observation is -

CHAIRMAN THEODORE: Please state your
name.

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MR. SCHWARTZ: Oh, excuse me, Don Schwartz, Palmetto Hall.

When I was working one of the things that we did was to what we call benchmarking; you make sure you knew what your competition was doing at least when you design a product so that you could at least be competitive.

One of the things -- if you just make an observation you can do this yourself. All of the plantations in this area that have a berm were all done by commercial developers. They obviously understood something about the way to protect both the visual and the noise impact that would emanate from the surrounding roadways. They didn't plant trees, they didn't plant as you -- an new plant, a buffer by putting a variety of vegetation, they clearly understood the value of a berm because it obliterates all visual impact from outside of the area.

Now, those are commercial developers. Now, we have -- we're in

1 the public arena, wherein the
2 government is now deciding how they're
3 going to mitigate this visual impact as
4 well as the noise and what we've got
5 we're going to plant trees.

6 What I'm suggesting to each of
7 you, because I've heard some comments
8 up here from some of you talking about
9 the things that might be planted on top
10 of a berm, one of the best berms as you
11 go out driving off the island -

12

13 CHAIRMAN THEODORE: Let me -- let me add
14 that the berm has already been brought
15 up a couple of times so are you -

16

17 MR. SCHWARTZ: I'm going to ask
18 the question, try to make a
19 recommendation in a moment about what
20 could be planted on the berm.

21

22 CHAIRMAN THEODORE: Okay.

23

24 MR. SCHWARTZ: And I'm suggesting
25 that you take a good look at the one at
26 Moss Creek. I don't know -- there's no

1 trees planted on that, that's some kind
2 of a vine or an ivy and my
3 recommendation to all of you is that
4 maybe you all ought to do some
5 benchmarking, drive around and look at
6 what is being done to mitigate the
7 noise and the visual impact on other
8 plantations in this area, very easy to
9 do, just get in your car and drive
10 around. Thank you very much.

11

12 CHAIRMAN THEODORE: Thank you. Sir?
13 Please add your name to the list.
14 State your full name.

15

16 MR. DAN DAVIS: My name is Dan Davis.
17 I'm General Manager of Port Royal
18 Plantation. I have a practical
19 concern.

20 In reviewing the mitigation plan I
21 notice that the predominant species
22 that is suggested is Little Gem
23 Magnolia. In my 16 years at Port Royal
24 we've planted a lot of Little Gem
25 Magnolias, we don't plant them anymore
26 and that's because their survival rate

1 is fairly abysmal. As a matter of fact,
2 land owners who apply for them now we
3 discourage them from using them. They
4 don't die in the first year, which is
5 good for the contractor, but they do
6 have a tendency to get scale after
7 about a year because of their weakened
8 condition in the transplanting and
9 after about three years they're --
10 they've lost most of their leaves and
11 we found that they just -- they don't
12 make it, most of them don't, so I would
13 encourage you to look at a substitute
14 species for the Little Gem. I mean as
15 nice as a Little Gem is a ten-foot
16 Little Gem does not transplant well, at
17 all.

18
19 CHAIRMAN THEODORE: Have you seen
20 alternatives to the Little Gem that
21 seem to work well in Port Royal?

22
23 MR. DAVIS: As Mr. Parker said
24 we're not here to design -

25
26 CHAIRMAN THEODORE: No, I'm asking a

1 question. Based on your experience is
2 there an alternative to -- because
3 there's numerous varieties of Magnolias
4 that are like the Little Gem that --
5 you know I have one growing my yard
6 that does quite well.

7
8 MR. DAVIS: I stay away from the
9 Magnolias in general unless you were
10 planting them at a smaller size, you know,
11 they all have the scale -- the tendency
12 for scale. I would encourage you to
13 use more than four species that you're
14 -- that are suggested in this plan and,
15 you know, the Wax Myrtles as much as
16 I'm not a fan of Wax Myrtles they're
17 survivors and they're great for
18 buffers, there's several kinds of Cypress
19 that would be appropriate. There's
20 several kinds of holly that you could
21 use that are native and don't grow
22 too fast and make up a good buffer.

23
24 CHAIRMAN THEODORE: Thank you. Any other
25 citizens? Ma'am, would you like to
26 come up, please? And again, I'd like

1 to request that -- try not to duplicate
2 from the comments that have already
3 been stated.

4

5 MS. LANE: My name is Donna Lane.

6 I'm a new resident here to Hilton Head

7 Island. I don't live in a plantation.

8 I do live in the flight path of the

9 airport and I moved up there in that

10 area because I have a view of Port

11 Royal Sound as well as Fish Haul Creek

12 and it is spectacular.

13 It saddens me every single day when I have

14 to drive down Beach City Road and see

15 what has happened there. I don't have

16 the answers. I've been following, in

17 the library, everything that's been

18 going on here. I know you have your

19 job cut out for you but I've never

20 smelled jet fuel before until after the

21 trees were removed. I don't want to

22 see runway lights when I drive down

23 Beach City Road in the morning or in

24 the dark. And I've listened to chippers

25 for months and months in the morning

26 from 8:00 o'clock until in the

1 afternoon. I don't want to hear that
2 anymore. I don't want to hear grasses
3 that have to be cut with chippers, so I
4 don't have answers for you, I wish
5 there were but I know it's emotional
6 and I certainly you will hope that
7 you'll take that into consideration for
8 the people who don't live in a
9 plantation but love my view and I
10 don't want to move. Thank you.

11

12 CHAIRMAN THEODORE: Thank you. Any other
13 citizens? Ma'am, please come forward.

14

15 MS. SHADE: My name is Judith
16 Shade. I am the President of the
17 Landowners Association of Port Royal
18 Plantation. I am not here representing
19 Port Royal Plantation, I'm here as a
20 24-year resident of an island that is
21 designated as Tree City USA for the
22 last nine years. It's on our website
23 if you are aware of it. I do not want
24 to see us lose that designation. I
25 would not like to see any of us be
26 responsible for losing that

1 designation. I think it's a wonderful
2 thing that we should be very, very
3 proud of.

4 I don't know whether there are
5 restrictions on the berm when you're
6 talking about base versus altitude but
7 I do know when during the construction
8 of the Cross Island and in various
9 places that we've lived outside of
10 South Carolina there are things called
11 sound barriers, brick, different kinds
12 of concrete material, sometimes CMUs,
13 concrete masonry units, which are put
14 in, you let ivy grow out of them,
15 they're great sound barriers, they're
16 probably more expensive short-term as a
17 berm or as a protective sound barrier
18 along Beach City Road but they would be
19 a much less expensive long-term
20 because, you know, CMUs don't require
21 irrigation. That's all I have to say.
22 Thank you very much.

23
24 CHAIRMAN THEODORE: Thank you. Any other
25 citizens? All right. Do we want to -
26

1 VICE CHAIRMAN SODEMANN: Can I ask a
2 question of the consultants that were
3 originally up here or -- I hear a lot
4 of people asking about berms and things
5 along that line. Has that been a
6 consideration and if so what was the
7 thought process behind that or -

8
9 MR. STEARNS: No, it is
10 not. We basically had no grading on
11 this project so we've not grading any
12 areas, including adding berms. The
13 berm is, you know, certainly understand
14 what their concern is about the visual
15 screening. I'm not a noise expert
16 though so I couldn't speak to the noise
17 issues regarding berms versus trees versus
18 noise walls but we have not considered
19 berms because we didn't want to change
20 the land shape basically and eventually
21 this airport will expand and that area
22 is already in the object-free area of the
23 airport so in putting anything in there
24 it really is a violation of the FAA
25 space but because of the Town ordinances
26 dealing with the buffers, you know,

1 we're required by the LMO to mitigate
2 that with planting trees and there's no
3 requirement to mitigate with earth
4 buffers or earth mounds so we basically
5 met and would we get credit for that if
6 we used an earth berm rather than
7 planting trees.

8
9 CHAIRMAN THEODORE: But you're saying if
10 they expand in the future that's a what
11 you call it an O -

12

13 MR. STEARNS: Object-free
14 area.

15

16 CHAIRMAN THEODORE: Object-free area.
17 Does that include the buffer, as well,
18 that eventually that would have to go?

19

20 MR. STEARNS: Right. You
21 can see it encroaches on the runway
22 center line so.

23

24 CHAIRMAN THEODORE: All right. And had
25 you taken into consideration the fact
26 that there's a 15-foot easement running

1 through that buffer which is minimizing
2 the width of the actual vegetation?

3

4 MR. STEARNS: We worked with
5 the Town on defining what that easement
6 width was in concert with the Town and
7 as we understood it that's normally
8 associated with a 50-foot buffer, this
9 one was expanded to 75 feet around the
10 airport property. There was never --
11 this -- that was never discussed in our
12 dealings with the Town that it would be
13 an additional 15 feet because of the
14 sanitary sewer easement.

15 This -- for this specific as we
16 understood it this amended LMO was just
17 a straight 75 foot buffer from the
18 property line for the adjacent use.

19

20 CHAIRMAN THEODORE: All right. Is there
21 any other questions of this gentleman
22 or discussion? I'd like to hear from
23 everybody. Mr. Smith?

24

25

26

1 MR. SMITH: No. I've heard it
2 all.

3
4 CHAIRMAN THEODORE: Ms. Moffett?

5
6 MS. MOFFETT: Not really sure.

7
8 MS. WELCH: Is there a legal issue with the
9 75 feet that's pending? Is there a legal issue
10 pending on the -- whether it should be
11 60 feet or 75 feet?

12
13 CHAIRMAN THEODORE: Would you be able to
14 answer that, Mike, if -- what you're --
15 you know, how the -- this whole thing
16 had progressed to this point in defining that
17 75-foot and having that sewer easement
18 run through there.

19
20 MR. ROAN: I'm going
21 to refer that to Teri Lewis. She's our LMO
22 Administrator. She's been the point
23 man on the airport project since the
24 Wright brothers so I'm going to turn it
25 over to her.

26

1 MS. LEWIS: Hi. Teri Lewis for
2 the record. There is not a legal issue
3 with it. This is what was set out and
4 what was adopted in the LMO as the
5 75-foot buffer. We knew that there was
6 a sanitary sewer easement that ran
7 through there. We anticipated that
8 there would be heavy plantings on
9 either side of that sanitary sewer
10 easement.

11 I've also been directed to speak
12 with the Hilton Head Public Service
13 District and see if there's any way
14 they would allow some portion of that
15 15-foot easement to be planted. We
16 know the entire area wouldn't be but
17 sometimes they'll allow a couple of
18 feet, you know, maybe even up to five
19 on either side of the line to be
20 planted, we're not sure of the answer
21 to that yet.

22
23 CHAIRMAN THEODORE: Had the 75-foot, when
24 that was defined, that was defined with
25 the intention of having the sewer
26 easement in that?

1 MS. LEWIS: That sewer easement's
2 been there for a long time.

3
4 CHAIRMAN THEODORE: Oh, I know but when
5 they were establishing saying, well, we
6 need a 75-foot buffer, was that based
7 on the knowledge that there was already
8 an easement there that was going to
9 take up 15 feet of the subject
10 property?

11
12 MS. LEWIS: They knew that the
13 easement was in there, yes.

14
15 CHAIRMAN THEODORE: Okay.

16
17 MR. PARKER: Mr.
18 Chairman, may ask a question?

19
20 CHAIRMAN THEODORE: Sure.

21
22 MR. PARKER: Is the
23 requirement of 75-foot buffer as
24 written?

25

26

1 MS. LEWIS: Yes, it says the LMO
2 states -

3

4 MR. PARKER: Adjacent to
5 this roadway?

6

7 MS. LEWIS: Yes, the LMO states -

8

9 MR. PARKER: Adjacent to
10 this roadway is the buffer required to
11 be 75 feet.

12

13 MS. LEWIS: A minimum of a 75-foot
14 buffer, adjacent use buffer and
15 adjacent street buffer that's what the
16 LMO states.

17

18 MR. PARKER: My experience with buffers
19 in the 28 years
20 I've been designing on the island has
21 been that the Town -- you're not
22 allowed to do anything in the buffer
23 unless it's perpendicular and if it's
24 parallel through the middle of the
25 buffer I can see that as an issue and I
26 think it would be an issue most of the

1 time in a 75-foot required width if you
2 take 15 or 5 doesn't matter out of that
3 buffer parallel, I see that as an issue.

4

5 CHAIRMAN THEODORE: Yeah.

6

7 MR. PARKER: Typically
8 it comes up as an issue. Has there
9 been any discussion why it's not an
10 issue?

11

12 MS. LEWIS: No. I guess what I
13 would point out is that, you know,
14 originally there was no 75-foot buffer
15 there it was the regular buffers that you
16 had -

17

18 MR. PARKER: -- 75 foot
19 buffer? Is it written?

20

21 MS. LEWIS: The 75-foot buffer was
22 adopted with all the airport amendments
23 in 2010.

24

25 MR. PARKER: Right.

26

1 MS. LEWIS: Prior to that it was
2 the typical buffers that were already
3 there. It was a 30-foot buffer from
4 Beach City Road, that easement was
5 already there so in all that time the
6 easement hasn't been newly cleared,
7 there was always -- you know and I say
8 'always' I mean you know - 5 to 10 years.

9
10 MR. PARKER: This new
11 work that's being done though is being
12 looked at as new work and now we're
13 mitigating and the rule in place is
14 75 feet, is it not?

15
16 MS. LEWIS: It says that there
17 should be a minimum of 75 feet.

18
19 MR. PARKER: Minimum of 75 feet. Okay.

20
21 MR. ROAN: I think --
22 I'm going to drill down maybe because
23 I've done it from your side of the
24 desk, too.

25 MR. WILLIAMS: Mike, can you speak
26 up, please?

1 MR. ROAN: When it was -- when it
2 went from 30 to 75 in 2010 as a part of
3 the airport work it was done with the
4 knowledge that the easement was in
5 place already and I mean it's -- the
6 math was derived with the easement
7 there I guess is what -- to maybe
8 answer your question a little more
9 directly but I don't know if it does or
10 not.

11
12 CHAIRMAN THEODORE: Are there places where
13 there's a 75-foot buffer and there are
14 no easements running through it?

15
16 MR. ROAN: On the Island or in the
17 subject property?

18
19 CHAIRMAN THEODORE: In the subject
20 property. I mean does the easement run
21 through the entire length of that
22 buffer or?

23
24 MR. ROAN: The easement's split right
25 here. You can see on this property
26 line with the church 7 and-a-half and 7

1 and-a-half approximately, it's part of
2 the 75 feet and that extends all the
3 way through then through the church
4 property onto Dillon Road.

5
6 CHAIRMAN THEODORE: Uh-huh.

7
8 MR. ROAN: It's free and clear 75 here
9 and then 75 all the way through the
10 easement and 15 feet through here in
11 down there.

12
13 CHAIRMAN THEODORE: So there are -- there
14 is precedence that there is 75 feet of
15 full vegetation in some of this area?

16
17 MR. ROAN: Along Dillon Road that
18 would be correct.

19
20 CHAIRMAN THEODORE Okay.

21
22 MR. WILLIAMS: -- adjacent
23 use buffer and the well site -

24
25 MR. WILLIAMS: May I, Mr. Chairman -
26 look at page 5B of the

1 plans. Mike, can you turn this
2 overhead on. Thank you.

3 There's a match line between
4 sheets 5A and 5B you can see the 75 --
5 the 15-foot easement through the buffer
6 here along the boundary line of St.
7 James Church, here's the match line
8 picks up here. You can see the sewer
9 easement continues here along St.
10 James Baptist Church and it continues,
11 this tract here, which is either a lift
12 station or a well site owned by PSD
13 Number 1. The adjacent use buffer
14 along this boundary line, this boundary
15 line and the adjacent street buffer
16 along Dillon Road does not have that
17 easement in there and you've got the
18 full 75 feet of buffer area there to
19 use.

20 Now, I don't know why it seems to
21 show a strip still through here when
22 there is no plantings but there is no
23 easement there, so Mr. Chairman, to
24 answer your question, yes, there are
25 areas shown on these plans where the
26 full 75-foot depth buffer is available

1 but in other areas it is not because of
2 the existence of that easement.

3

4 CHAIRMAN THEODORE: Thank you.

5

6 MS. WELCH: I have
7 another question, Mike. If there -- is
8 there anything on record that would not
9 allow a berm? Are there issues that
10 would prohibit designing a berm?

11

12 MR. ROAN: Administrative issues
13 I would say no. I don't think there's
14 anything in the books that says you can't
15 do a berm. I think there's some physical
16 constraints on the site but that's
17 without doing any research on it.
18 It's fashionable today to say what you're not - I'm
19 not a civil engineer, so.

20

21 CHAIRMAN THEODORE: Do you know, Mike, if
22 the State Historic Preservation Officer
23 if that would be fully -

24

25 MR. ROAN: I would say they'd be
26 highly interested in us putting a berm

1 on top of stuff but I can't say with authority
2 on that either. That would
3 certainly be the first telephone call I
4 would make if we were contemplated a
5 berm.

6
7 MR. GARTNER: Mike, can I
8 clarify? On the replacement trees
9 going back into 1294.

10
11 MR. ROAN: Yes, sir.

12
13 MR. GARTNER: I'm just
14 trying to clarify, with the inches
15 taken out as opposed to trees taken
16 out, are we saying that we're actually
17 mitigating -- we're putting in more
18 trees than actual trees were taken out?

19
20 MR. ROAN: More -- yeah, there's more
21 stumps going in the ground than what
22 was taken out.

23
24 MR. GARTNER: Okay.

25
26 MR. ROAN: Correct.

1 That is an accurate statement.

2

3 CHAIRMAN THEODORE: And Mike, there was no
4 funds directed towards a tree
5 mitigation bank?

6

7 MR. ROAN: I don't know.

8

9 MS. LEWIS: There were
10 not.

11

12 MR. ROAN: There were not.

13

14 CHAIRMAN THEODORE: Okay.

15

16 MS. LEWIS: They
17 managed to meet it all on one site. That was an
18 option for them if they couldn't meet
19 all of their mitigation on site.

20

21 CHAIRMAN THEODORE: Okay, all right. Mr.
22 Williams you want to come back up
23 again?

24

25 MR. WILLIAMS: With respect to the
26 issue earlier about whether or not the

1 existence of the easement when all this
2 was being talked about in May leading
3 up to the May of 2010 I think I was at
4 probably every one of those meetings, I
5 don't ever recall the Town staff
6 pointing out that particular issue,
7 either to the Town Council or the Planning
8 Commission or anybody else that, oh by,
9 the way the 75-foot buffer really is
10 the 75 feet in some areas this is only
11 60 feet. Thank you.

12

13 CHAIRMAN THEODORE: Thank you. Would the
14 applicant please come back up again? Oh,
15 I'm sorry, Mr. Sodemann?

16

17 VICE CHAIRMAN SODEMANN: But it was
18 originally a 30-foot buffer along that
19 road and then -

20

21 MR. ROAN: Right.

22

23 MR. SODEMANN: Okay.

24

25 MR. ROAN: 30-foot
26 buffer on Beach City Road.

1 VICE CHAIRMAN SODEMANN: Inclusive
2 of that 15 foot that's there for the
3 sewer?

4
5 MR. ROAN: I think
6 it's 15 feet between the property line
7 or the right-of-way line and the
8 easement.

9
10 MR. STEARNS: I would
11 like to add one point of clarification
12 if I might. In the area, this vacant
13 area right through here that -- well, I
14 don't see that's -- that's not -

15
16 CHAIRMAN THEODORE: On the
17 second sheet here?

18
19 MR. STEARNS: Yeah, the
20 one that the attorney spoke to. The
21 reason that's in there we thought it
22 would be prudent to leave some sort of
23 fire lane up through that that sanitary
24 sewer easement services wanted the
25 buffer along Beach City Road and that
26 would serve as fire access in the

1 future, however, if that's not an
2 important issue I mean that could be
3 planted. We could move plants from
4 areas that are deeper in the buffer
5 closer to the road if that is not an
6 issue. We thought it might be.

7
8 CHAIRMAN THEODORE: Well, I think the
9 intent it may be outside that buffer
10 area but it seems like that for the
11 purpose of, you know, the buffer that
12 you would want a full vegetated depth
13 as the ordinance was saying it was a
14 minimum.

15
16 MR. STEARNS: We can
17 certainly make that amendment.

18
19 CHAIRMAN THEODORE: And if you can do that
20 it seems like it would be just as
21 accommodating to compensate for the
22 15-foot sewer line easement on the
23 other side because it's sort of saying
24 the same thing although it would change
25 the footprint a little bit, instead of
26 being 75 it would be 90 feet from the

1 right-of-way line. Do you know, is
2 that an issue with the zones that are
3 showing up on the plan? It sounds like
4 it's not because your clearance height
5 isn't an issue with having that buffer
6 if it became -

7
8 MR. STEARNS: Yeah. That
9 has to do with respect to the distance
10 from the end of the runway, that is
11 correct, it would not be as much of an
12 issue.

13
14 CHAIRMAN THEODORE: And I know you
15 addressed this a little bit. Had there
16 been any discussion through this whole
17 process about berming?

18
19 MR. STEARNS: No, there
20 is not.

21
22 CHAIRMAN THEODORE: Okay. Had you
23 considered that as part of your
24 recommendations for -

25
26 MR. STEARNS: No, sir, we

1 did not. That would have removed
2 additional trees and we were asked and
3 even had an arborist onboard to make
4 sure that even trees that were pruned
5 that we had called for to be pruned in
6 the design would be salvaged if at all
7 possible when they -- once they mark
8 the pruning level on the trees so we
9 were able to save a lot more trees than
10 we had originally anticipated. A berm
11 most certainly would have taken more trees.

12
13 CHAIRMAN THEODORE: But considering that
14 you're contemplating that 15 foot extra
15 width that we're just talking about,
16 that could potentially be a bermed area
17 that there are no trees there now?
18 Correct.

19
20 MR. STEARNS: Sir, I would
21 have to check on air space requirements
22 and find out if that would be an issue.

23
24 CHAIRMAN THEODORE: Okay. But when you
25 say that, if that slide rule, you know,
26 what Mr. Williams is saying is that

1 degradation and if it's saying 40 feet,
2 50 feet in this area that shouldn't be
3 in conflict with -

4
5 MR. STEARNS: Well, there
6 are other issues, too, within the
7 runway safety area and part of this
8 falls in what they call the
9 obstacle-free zone and the runway safety
10 area and frankly I would to go back and
11 look at that to assure myself that that
12 would not be an issue or where the berm
13 might be put.

14
15 CHAIRMAN THEODORE: Okay. Is there --
16 just out of curiosity, were you
17 proposing that that existing fence that
18 runs across into the runway area, is
19 that going to be removed?

20
21 MR. STEARNS: Yes, sir,
22 that was going to be removed and the
23 fence extended to the buffer lines.

24
25 CHAIRMAN THEODORE: Okay.

26

1 MR. STEARNS: With a
2 black vinyl eight-foot chain link.

3
4 CHAIRMAN THEODORE: All right. Now when
5 you say that, that you're -- I don't
6 think the document said the black but
7 you're comfortable with doing it black?

8
9 MR. STEARNS: Yes, the
10 airport had asked us to specify black
11 vinyl, that will be in the
12 specifications.

13
14 CHAIRMAN THEODORE: Okay. Any more
15 discussions with this gentleman? All
16 right. Thank you.

17
18 VICE CHAIRMAN SODEMANN: I see a new fence coming along here
19 along Beach City Road at that 75-foot
20 point. Is that just strictly because
21 of this establishment of the 75-foot
22 buffer or can that -- is it movable?
23 Is it etched in stone? Does anybody
24 know the answer to that question or?

25
26 CHAIRMAN THEODORE: Would you mind coming

1 up back again? You understand that if
2 the buffer that's showing up on this
3 image right now grew to compensate for
4 that 15-foot sewer line easement that
5 would move your fence line. Is that
6 fence line in conflict in moving it
7 with any other clearance criteria?

8
9 MR. STEARNS: I do not
10 know at this time. I would have to look
11 into that.

12
13 CHAIRMAN THEODORE: That would be the same situation as if there
14 was a berm there kind of the same, to
15 clarify?

16
17 MR. STEARNS: Yeah.

18
19 CHAIRMAN THEODORE: Okay.

20
21 MR. STEARNS: Not exactly
22 but similar, yes.

23
24 CHAIRMAN THEODORE: Okay. Any other
25 questions for this gentleman while he's
26 up here? Thank you.

1 Let's start some dialogue on this
2 thing.

3 How do people feel about
4 compensating for the sewer line
5 easement running through there?

6
7 MS. WELCH: I think
8 there a couple of issues here that
9 would be beneficial to have clarified
10 for us before we try to make a decision
11 and one is the -- whether the 15 feet
12 should be looked at, whether the
13 potential for a berm is there and their
14 reclaimed 15 feet and whether or not
15 that fence can be moved; these are
16 questions that we can't answer and I
17 think would be nice -

18
19 MR. PARKER: If the
20 experts don't know how are we suppose
21 to?

22
23 MS. WELCH: Yeah.

24
25 MR. PARKER: Yeah.

26

1 MS. WELCH: And then
2 the other thing that came up during
3 this was the fact that the FAA
4 recommended something other than what
5 was going to be planted in that open
6 area, I, for one, feel like the federal
7 board has kept us safe in this country
8 and I would -- if they're recommending
9 something I'd like to know what their
10 options are, whether it's just Bermuda
11 grass or whether there are other things
12 because I think even though safety
13 isn't our purview here it concerns me
14 as a citizen.

15
16 CHAIRMAN THEODORE: Well, we can also, you
17 know, as part of the LMO says that it needs
18 to be native species and as far as
19 we're concerned, you know, if we're
20 comfortable in what the LMO says, one
21 of the options is native grasses is
22 that we can recommend that, you know,
23 that it needs to be that and obviously
24 there can be that, you know, reply back
25 from the airport or FAA, you know, with
26 alternatives that would meet that

1 criteria that would be less, you know,
2 inviting to birds and -

3

4 MS. WELCH: Right.

5

6 CHAIRMAN THEODORE: -- wildlife stuff.

7 You know, my personal opinion is I
8 think it ought to be native materials
9 that are used in there that -- that
10 they're not allowed to change the
11 grades in there so it's a very rough area
12 and to think that Bermuda could be
13 planted in there and mowed and all that
14 just seems -

15

16 MR. PARKER: It's not
17 going to happen.

18

19 CHAIRMAN THEODORE: No.

20

21 MR. PARKER: No.

22

23 CHAIRMAN THEODORE: So it's better to
24 naturalize it but use it, you know,
25 wisely in that it would be in keeping
26 with something that the FAA can live

1 with but as far as the ordinance it
2 says that that area would be replanted
3 with a low native vegetation.

4 You know, my opinion from the
5 aesthetics with that, that 75-foot
6 buffer is -- you know, I think in, you
7 know, my personal opinion is that, you
8 know, you look at the other road and
9 it's a 75-foot depth buffer and I think
10 that, you know, this area if the
11 applicant is comfortable with it is to
12 say why not make it 15 feet wider and
13 thicken that vegetation along that
14 corridor so that the easement for the
15 sewer line can be maintained but you
16 still get that full depth of vegetation
17 aesthetically. I think that's better.

18
19 MR. PARKER: An
20 equivalent depth of 75 feet in buffer
21 material is what we need there.

22
23 CHAIRMAN THEODORE: Right.

24
25 MR. PARKER: That's what
26 we need as a starting place and that

1 seems pretty simple.

2

3 CHAIRMAN THEODORE: It almost I guess
4 where we are is I don't know how the
5 applicant feels. I know they're rushed
6 trying to do this thing but we'd also
7 like to see this done right, as well,
8 is that it'd be worthwhile to take a
9 look at these. Mr. Kubic you want to come
10 up, please?

11

12 MR. PARKER: If I may
13 before -- the berm is still a really
14 big question for us I think because we
15 don't have an answer whether it's
16 possible or not and there are a couple
17 of good things that can come from that.
18 So, you know, was it considered?
19 Wasn't it considered? The designer
20 said it wasn't considered, seems like
21 it -- if I were designing this it would
22 be one of the considerations so I'd
23 like to know why it wasn't considered.

24

25 MS. MOFFETT: If I may
26 also add something onto that. Maybe,

1 you know, we talked a little bit about
2 Mitchelville as it relates to its
3 historic qualities, maybe if there's some
4 sort of advisory opinion as it relates
5 to the berm.

6
7 MR. PARKER: Does the
8 berm help that situation?

9
10 MS. MOFFETT: Yeah. Exactly.

11
12 MR. PARKER: And it
13 does. It sounds like it does.

14
15 MS. MOFFETT: I mean if
16 we're going to be talking about
17 aesthetics, I mean history is -

18
19 MR. KUBIC: My first comment would
20 be to answer your question next, just
21 thinking aloud and not realizing it
22 but the easement is for sanitary
23 purpose. I was just curious as to
24 whether or not anybody bothered to ask
25 or could ask the PSD that if we're
26 talking about native grass being a

1 possibility as a plantback I think the
2 clear easement was designed to prevent
3 trees from being put there.

4

5 MR. PARKER: Right.

6

7 MR. KUBIC: Now, hypothetically I
8 don't know and before I get accused of
9 being crazy or doing something way out
10 of line I think the first step would be
11 to see is there any alternatives that
12 could be planted on top of it other
13 than trees that would be amenable to
14 the sewer authority that controls the
15 easement because obviously tearing out
16 a grass plant versus a tree there may
17 be some difference and there may be
18 some cost savings associated with
19 working within that limit but seeking a
20 different option to satisfy some of the
21 concerns that were raised.

22 The other question about whether
23 or not a berm was considered was
24 primarily answered by the fact that the
25 FAA had indicated that if you do want
26 to consider a berm that has to be a

1 hundred percent funded by the County
2 and so the FAA is supporting a
3 replantation but if we decided to go
4 with a berm then we get into issues as
5 to is it funded by airport revenue?
6 Airport revenue bonds with the County
7 as a whole have to float or go into
8 debt, maybe the Town would fund the
9 berm with its funds but the issue was
10 there was no federal reimbursement for
11 it, so the first choice was to go with
12 what was payable from an outside source
13 and I think logically that that's why
14 we're here today.

15
16 MR. PARKER: That makes sense.

17
18 VICE CHAIRMAN SODEMANN: I think
19 we've raised a lot of questions and
20 probably need some more information to
21 really make a reasonable decision
22 but -

23
24 MR. ROAN: I'm kind of creating a laundry list
25 of issues for these guys to go back and
26 address.

1 CHAIRMAN THEODORE: Right.

2

3 MR. ROAN: Just to

4 expedite this.

5

6 MR. KUBIC: Can I sit down? Are you telling me to go sit

7 down?

8

9 MR. ROAN: No, please

10 don't. I can speak with a good amount

11 of certainty. I wish somebody from the

12 PSD was here to say that berming on top of the

13 easement is a non starter so that puts

14 you either on the front side of it or the back

15 side of it, and frankly to put it on the front side of it makes any of

16 the stuff you put

17 on the back a moot point you're putting

18 the elephant in front of the mouse.

19

20 MR. SMITH: I can't hear you,

21

22 MR. ROAN: You haven't

23 missed much. So, the berm is

24 this a function of sound or is this a

25 function of buffer? I mean the first

26 thing I'd want you to clarify for them

1 because that's going to affect the
2 height, not only are you talking about
3 height you're talking about pitches.
4 The front corner and the back corner of
5 this thing's about two-to-one grade. At which point
6 you're affecting the amount of plants
7 you can put on it. You know, everyone
8 keeps throwing their own design anecdotes
9 on this thing. I used to design for a
10 golf course architect and legally three
11 to one is about as tall as you can go
12 which is a little taller than this, so
13 two to one is about as aggressive as
14 you can get. If it's a function of noise
15 they probably need to know that because -
16 four feet along the airport - it's going to be a great buffer for the
17 airport for all the cars going by but
18 it's not going to help anybody with the plane
19 noise, especially at Port Royal or
20 Palmetto Hall and let me preface it, I
21 have no emotional attachment to this
22 whatsoever, I want to get a permit
23 issued on this project.

24 The second is no one's discussed
25 the main thing we're supposed to be
26 discussing and that is, is the plant

1 material, the size, the quantities and
2 locations sufficient to buffer the
3 site? I'd hate for this to come back
4 and there's possibly a berm and then
5 all the tree material is -- isn't
6 sufficient or what they're putting back is not
7 sufficient. If we could provide them
8 with some guidance on that, as well.

9 The grass, if this thing works the
10 way it's supposed to work no one's
11 going to see the grass on the inside
12 anyway. I've got my own personal
13 preferences on the grasses because of
14 native species and storm water
15 quality but it's a little -- it's -- it
16 rings a little hollow that everybody's
17 concerned about the affects of the
18 grasses on the buffer. There are
19 concerns that are valid for it, the
20 visual quality isn't one of them
21 because if the buffer's going to meet
22 anybody's satisfaction you will never
23 see the grass anymore.

24 All these points is not trying to
25 minimize anybody's questions but when
26 these guys leave to answer these

1 questions I think they need to know the reasons
2 why, what the end game is. Is it noise
3 versus buffer on the berm because the
4 berm then the berm would have to be
5 back here, at least that gives them a
6 lateral dimension on which to start
7 pitching stuff and that's going to
8 probably adversely affect the amount of
9 plants you can put in the ground.

10 If it's noise that's a whole different animal and
11 then second somebody could speak to
12 and, you know, Todd, you're the
13 landscape architect on the Board you're
14 probably going to have to carry a lot
15 of weight on this, the types of
16 materials that are going in place and
17 do you feel that just as material is
18 sufficient to buffer it visually and
19 I'll leave it at that but I just want
20 to make sure everybody's walking out of
21 here with a set of marching orders to
22 actually get them to their destination.

23
24 CHAIRMAN THEODORE: And I agree, Mike,
25 with what you said you know, the -- but
26 the one thing is the LMO that had been

1 amended for the Airport Overlay
2 District says that in terms of this
3 lower cut area -

4

5 MR. ROAN: Uh-huh.

6

7 CHAIRMAN THEODORE: -- it needed to be
8 native vegetation.

9

10 MR. ROAN: That is
11 completely true a -

12

13 CHAIRMAN THEODORE: You know, Bermuda
14 grass is not a native grass so
15 aesthetically, you know, that would be
16 a better recommendation especially for
17 the condition that the soil and the grounds
18 are in right now as we speak, you know,
19 so. And as a landscape architect I could say that's
20 a good solution is to find the right
21 native grasses that could be planted in
22 there and hopefully they're low
23 maintenance as well so it wouldn't
24 require a high amount of care to
25 maintain its integrity. You know, I'm
26 struggling with the berm because I know

1 that the examples you know I know
2 all those. The elaeagnus that's growing on
3 the ones in Moss Creek and those were
4 road noises from the cars, you know,
5 we're talking about airplanes and
6 they're already taking off and they're
7 already up in the air in this area so
8 I'm not an expert I'm just not sure
9 what that does from a noise abatement,
10 maybe it helps with a visual but we're
11 also talking about a vegetative buffer
12 is supposed to be a visual barrier, as
13 well, and I've seen examples of the
14 trees that have been pruned and I think
15 why they were out there the entire time
16 these trees were being monitored as
17 they were pruning them with an arborist
18 and they were telling them where the
19 cut points were and in some cases they
20 were saying take the tree out because
21 by the time you cut to that controlled
22 height that tree isn't going to survive
23 so some of them were taken out and
24 then, you know, you can look and see
25 where those little branches up there
26 where they've pruned them and I've seen

1 examples of where it had been pruned I
2 don't know how many years ago, 15, 20
3 years ago and you could see where those
4 trees had grown back so I like the fact
5 that they kept those trees and are
6 giving the option for something of
7 height to reestablish itself and these
8 are obvious native trees and the roots
9 are already in the ground so, you know,
10 I think those are going to become
11 important elements, trees in the future
12 to revegetate that area, so what is
13 being proposed is below that and is
14 that, you know, that ground level up to a
15 20-foot height say and the Wax Myrtles
16 I think will do a good job of filling
17 that in, the Yaupon. I don't know what
18 the Town's history is with the Little
19 Gems, I've never had problems with
20 those but, you know, that's just --
21 what's nice about that is it gives a
22 variation in vegetation material -- broad
23 leafed evergreen versus everything else is a
24 narrow leaf.

25

26 MR. ROAN: The reason

1 the Little Gem was selected is because
2 when we have any replacement trees for
3 any type of development, commercial,
4 government or whatever all of our
5 trees are done by categories. It took a lot
6 of Category One trees out and the
7 removal of these trees have to be
8 replaced with other Category One trees
9 that are live oaks, broad leafed
10 evergreen trees, live oaks, Magnolias,
11 you know, there's a list of about 12.
12 All of those don't meet the FAA
13 regulations about height - we said put
14 Magnolias back because they tend to
15 stay a little smaller and there's lots
16 of dwarf varieties. I can't go get you a dwarf
17 live oak, but I can get you a dwarf
18 Magnolia and that meets the
19 Category One and that gets back into
20 ecology and diversity and
21 things like that, that's the reason
22 that plant was selected.

23
24 CHAIRMAN THEODORE: And that was also on
25 your recommendations list?
26

1 MR. ROAN: It was on the recommendations list, as well.

2

3 CHAIRMAN THEODORE: Okay. So from the

4 type of material, you know, they're

5 rock hard, I mean they're -

6

7 MR. PARKER: At day one, your experience, day one these go in

8 the spacing and the sizes that they're

9 talking about, what does your

10 experience say that's going to look

11 like?

12

13 CHAIRMAN THEODORE: Well -

14

15 MR. PARKER: I know what it's going to be in five years,

16 but at day one what's it going to look like? Is it

17 sufficient? That's the basic question.

18

19 CHAIRMAN THEODORE: Those are

20 typically the materials that are specified for large materials for a

21 very wide buffer.

22

23 MR. PARKER: So they're

24 tall. How wide are they?

25

26 MR. ROAN: Funny you

1 should ask.

2

3 CHAIRMAN THEODORE: Did you get your pictures?

4 MR. ROAN: This rarely works but

5 sometimes it does. This is sort of the

6 e-mail I was waiting for.

7

8 MR. PARKER: Do you want to pass it around?

9

10 MR. ROAN: I don't have enough gum for everybody, so bear with me.

11 These are eight foot Yaupon

12 Hollies in the pot so they're about ten

13 feet tall, they're planted at about ten feet on

14 center, so that's an example of what

15 they're proposing to install.

16 That's a ten foot Wax Myrtle in

17 the ground. These are all nursery

18 shots by the way.

19

20 CHAIRMAN THEODORE: That's a Cedar?

21

22 MR. ROAN: This is a

23 ten foot Red Cedar, and that's a ten

24 foot Little Gem Magnolia.

25

26 MR. SMITH: It would be

1 pretty hard to see anything from the
2 road into the airport when they're
3 planted then.

4
5 MR. ROAN: I think -- oops, that's my mail, don't look.
6 I think that's pretty indicative
7 of how -- what you will see planted in
8 the field -- that's ten feet material,
9 planted ten feet on center, it's the
10 size, shape form, mass of what's going
11 to go in there.

12
13 MR. PARKER: And we're
14 talking eight feet on center?

15
16 MR. ROAN: And we're talking eight feet
17 on center.

18
19 MR. PARKER: Is that a
20 stagger? How will you do that in the
21 field can you explain that.

22
23 MR. ROAN: I can save
24 you a trip up if you want.

25
26 MR. PARKER: Yeah.

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MR. ROAN: These are
all in triangulation pattern -

MR. PARKER: That's all
I wanted to hear, triangulation
pattern.

VICE CHAIRMAN SODEMANN: These go
all the way to the ground so you're not
looking underneath anything here.

MR. ROAN: That is
correct.

MR. SODEMANN: You're
looking into the body of -

MR. ROAN: Now, do
your berm arguments and do your moving
the moving fence back arguments all
hold weight? Yeah, I mean that's
something I'd like to support, I think
they're all valid points but if it's a
function of noise I think it's a
probably no because that's going to be

1 a buffer -- it is a function of sight,
2 it's going to reduce that -- and this
3 is what you're going to get from it.

4

5 MR. PARKER: The berm
6 helps with noise, but that's not why it
7 was -

8

9 MR. ROAN: Sure.

10

11 MR. PARKER: -- why I was
12 envisioning it, it was more to give
13 amplitude to the initial planting.

14

15 MR. ROAN: Uh-huh.

16

17 MR. PARKER: And
18 visually it cuts it off visually from a car if
19 it's three, three and one half feet high.

20

21 MR. ROAN: Very true.

22

23 MR. PARKER: Especially
24 if it can't be above the right-of-way
25 and it can't be up against the road and
26 it's going to constant affect the trees

1 that are going in there, I'm not sure
2 it does, it's doing a lot for us. More
3 importantly I think we need 75 feet of
4 plantings not 60 feet of plantings.

5
6 MS. DEBORAH WELCH: Well, the
7 berm, issue, though, does help over
8 time if the plants die.

9
10 MR. PARKER: True.

11
12 MR. SMITH: The problem
13 is if you put a berm in it's going to
14 have to be on the inside -

15
16 MR. PARKER: Right.

17
18 MR. SMITH: Of the easement, so it's not going to --
19 it won't have the same effect.

20
21 MR. PARKER: Yeah.

22
23 MR. SMITH: If it were
24 further out towards the road.

25
26 VICE CHAIRMAN SODEMANN: If it's

1 there for sound control I don't think
2 you have any space to do anything
3 effective as far as the berm goes.

4

5 MR. PARKER: No, it'd
6 have to be a wall.

7

8 VICE CHAIRMAN SODEMANN: We're not
9 talking - that wouldn't work with the historic?

10

11 MR. PARKER: No. We have a wall of green. If
12 everything lives, it's irrigated it's
13 put in correctly it's fertilized,
14 maintained. They're all spaced out
15 right -- initially it's a wall. Five
16 years down the road you're not going to
17 have a clue that there's an airport back
18 there -- if it's done correctly. And that's what we're looking for.

19

20 CHAIRMAN THEODORE: And there is an
21 agreement as well that when this is planted - there
22 will be a visual inspection of that
23 buffer and if there's any weak areas
24 then those will have to be supplemented
25 with additional plantings.

26

1 MR. PARKER: After the
2 initial planting, yeah.

3
4 CHAIRMAN THEODORE: Right.

5
6 MR. PARKER: So irrigation is really critical.

7
8 CHAIRMAN THEODORE: I think
9 irrigation really is critical. It would be
10 good to know, as the applicant was saying, that it sounds like
11 there's a fairly strong contractual
12 agreement with the installer for a year
13 and then I think it would be good to --
14 and what Mr. Kubic had said was to at
15 least see, you know, that there's
16 agreement that there would maybe about
17 a three- year maintenance contract on
18 this buffer area that would extend
19 beyond that first year to know that
20 there is something in place, there is
21 funding in place, that they will
22 maintain this thing and continue to
23 make sure that there is water monitoring and,
24 you know, there is always the issue of
25 vines and other stuff that's going to
26 be growing up through here is to maintain

1 those so that the integrity of this
2 installed material can get fully
3 established as well so, you know, I
4 think that would go a long way and then
5 in addition, you know, we talked about
6 making sure that the rest of this area
7 in the corridor is more of a native
8 type planting and, you know, they could
9 come back with recommendations based on
10 that, just come back with us not with
11 an ornamental grass like a Bermuda but
12 come back with a native grass, or low-growing
13 native vegetation that would be acceptable to the Town's
14 requirements and that they would be
15 comfortable with as an airport safety
16 code requirement.

17

18 MR. SMITH: Agreed.

19

20 CHAIRMAN THEODORE: With that said, you
21 want to summarize recommendations and
22 have them come back with -

23

24 MR. PARKER: Well, didn't you just do that?

25

26 CHAIRMAN THEODORE: Well, did we miss

1 anything?

2

3 MS. WELCH: I'd like

4 them to consider to perhaps withdraw

5 the application and consider the berm

6 and whether that's at all feasible from

7 an aesthetic point of view I just think

8 a varied topography rather than straight

9 along the road would end up being

10 nicer. However I can certainly accept

11 the fact if that's not feasible, at

12 this point.

13 And the other issues were the

14 maintain -- some sort of maintenance

15 guarantee that it's going to last more

16 than a year. Whether native grasses will

17 be approved by FAA because there are a

18 lot of birds that like to nest in

19 native grasses so that is something

20 that I feel like they should be telling

21 us.

22

23 CHAIRMAN THEODORE: Well, I'm not sure

24 necessarily -- I guess the argument

25 that I was hearing is that they were

26 concerned about the fruit that was

1 attracting the birds that, you know,
2 the Bermuda grass left unattended, you
3 know, can harbor wildlife and birds, as
4 well, so, you know, I -- I think
5 there's a viable solution. Yes, sir?

6
7 MR. GRUBER: Good afternoon. Josh
8 Gruber, Staff Attorney with the County.

9 I believe in your packet is a copy
10 of the letter that the County received from
11 the FAA so you can see what it was that
12 they stated. I believe they looked at
13 the native grasses contained in the
14 Town's LMO and it was their opinion
15 that those would be an attractive -- as
16 you stated for the wildlife so their
17 only recommendation is Bermuda grass
18 but I just want to know -- I believe
19 that's in your packet that way you can
20 see for yourselves that they did --
21 that they communicated to the County.

22
23 CHAIRMAN THEODORE: But that's based on
24 the list that the Town had as -

25
26 MR. GRUBER: It is based

1 on the town's current LMO.

2

3 MS. LEWIS: Which I would say we
4 received a copy of that letter, too,
5 and we reviewed it.

6 The impression that we got from
7 reviewing that letter was that they
8 looked at what was in the existing LMO
9 not at what we proposed, which were
10 three native grasses, so they were
11 still looking at things like the
12 blueberries and other plants that
13 would, you know, would bear fruit so we
14 didn't think that they looked only at
15 grasses.

16 The other thing I want to point
17 out is to do anything other than native
18 vegetation that area will require an
19 LMO change, so -

20

21 CHAIRMAN THEODORE: And when you say that
22 Teri, are you saying like if they did
23 Bermuda?

24

25 MS. LEWIS: If they want to plant
26 Bermuda and that's all that will be --

1 that's all that will be considered we
2 need to change the Land Management
3 Ordinance first, the Land Management
4 Ordinance has been to be amended before
5 that can be allowed. Because right now we
6 state that clear cut
7 area has to have native vegetation
8 replanted in it but we said native
9 grasses were one of the choices for
10 that so if native grasses were planted
11 in there and we gave three examples of
12 ones that wouldn't bear fruit then that
13 could be approved without changing the
14 land management ordinance.

15
16 CHAIRMAN THEODORE: That's why
17 I kind of feel like what I know of the
18 LMO and in reading it that
19 aesthetically the native grass option
20 seems like a better solution even
21 though it's behind the buffer area it's
22 still a good use for that knowing that,
23 you know, the ground is still like a
24 forested ground. They weren't able to
25 really -

26

1 MS. LEWIS: Right.

2

3 CHAIRMAN THEODORE: -- change
4 the grades out there so it seems like
5 that's -- has a better viability of
6 reestablishing itself than trying to
7 introduce -

8

9 VICE CHAIRMAN SODEMANN: Things like
10 that left out there -

11

12 CHAIRMAN THEODORE: No, they're
13 right a Bermuda grass wouldn't work
14 they would all -- (Inaudible) -- stumps
15 and everything wouldn't.

16

17 VICE CHAIRMAN SODEMANN: Well it
18 sounds like we've got a number of items
19 that need to be addressed and --

20

21 CHAIRMAN THEODORE: Would the applicant like to
22 come up again?

23

24 MR. STEARNS: If the
25 decision is made that we need to go
26 back and consider an earth berm I'd like

1 a point of clarification on where that
2 berm would be considered because if
3 it's on the airport side of the buffer
4 where the trees have already been
5 removed it would not be visible from
6 the roadway and if it's on the roadway
7 side of -- against the right-of-way of
8 the road then it's going to displace
9 more trees that were left -- that
10 remain in there now, so we'd like a
11 point of clarification if that's the
12 decision that is made for us to
13 consider.

14
15 CHAIRMAN THEODORE: All right. Thank you.

16 You know one of the thoughts, you
17 know, if the Board is interested in
18 seeing a berm solution that, you know,
19 there's that supplement for that 15
20 foot of space where the sewer easement
21 is creating that void but I'm not sure
22 what you could put as a berm within
23 that area and you mentioned about it
24 being naturalized and stuff versus a
25 long linear berm. I don't know how
26 much flexibility they have with that

1 and I guess we could ask, you know,
2 that it would have to be on -- at least
3 I believe it would have to be on the
4 inside of where your 75 foot edge is
5 right now. If there was a berm
6 solution.

7
8 VICE CHAIRMAN SODEMANN: Right. It can't be up against the road.

9
10 MR. SMITH: You'd have to cut down the trees.

11
12 CHAIRMAN THEODORE: No, there
13 are no trees. They've already been
14 taken out beyond where they're showing
15 that 75-foot line so either way if they
16 expand the depth to be a full 75 feet
17 of planted vegetation they would have
18 to move 15 feet into that fully cleared
19 area or is that a place where they
20 could supplement with a berm, as well?

21
22 MR. PARKER: You're not
23 going to see it from the road if you
24 did it that way, it just -- you know,
25 what they're doing at that point.

26

1 MS. WELCH: Well hopefully - the rest of
2 the planting.

3

4 MR. PARKER: Other than
5 giving it a little height and plant on
6 top of it and then we're just going to
7 plant smaller bushes so.

8

9 CHAIRMAN THEODORE: Sir, would you like to
10 come back up again?

11

12 MR. ANDRES: My name is
13 Paul Andres. I'm the Airport Director.
14 Just two items of information I want to
15 share with you in your discussions. We
16 have made preliminary inquiries to the
17 FAA about the concept of berms, there
18 are a number of issues that would have
19 to be explored. Most notably is the
20 location of any berm in relation to the
21 safety areas at the airport and I
22 believe those safety areas are very
23 close to the areas that you're talking
24 about, right now.

25 And as far as the native grasses
26 in the cleared area those specific

1 examples were brought to the FAA's
2 attention that you're talking today and
3 they defer to the USDA wildlife
4 services experts here in the State of
5 South Carolina and their comments back
6 were that those types of plants create
7 edge habitat which is attracted to
8 wildlife and should not be considered
9 for that particular area and that's in
10 part in that letter from Scott Serritt
11 from the FAA.

12
13 CHAIRMAN THEODORE: But weren't, as Mike,
14 and you might clarify this, this zone
15 where you've done this ground clearing
16 has an elevation of -

17
18 MR. ANDRES: It's relatively level right
19 now, it's relatively level. It
20 undulates only a few feet basically.

21
22 CHAIRMAN THEODORE: Well, what's the
23 clearance height like where it says
24 40 feet.

25
26 MR. ANDRES: Well, 40 feet what you have

1 to understand that's an MSL altitude,
2 mean sea level altitude, the ground
3 elevation is normally 19 or 20 feet so
4 the tallest any object can be
5 underneath that line is 20 feet tall or
6 it encroaches into the air space and it
7 obviously gets shallower the closer you
8 come back to the runway.

9
10 CHAIRMAN THEODORE: And what's the
11 difference between doing native grasses
12 there versus the area beyond that 50
13 that has buffer vegetation in there
14 which will harbor birds and -

15
16 MR. ROAN: Well, it can potentially
17 harbor some birds but the area
18 immediately at the end there is an
19 extension beyond the runway safety
20 area, that little checkerboard area is
21 actually runway safety area that the
22 FAA wants planted only in standard
23 Bermuda grass plantings. They're
24 concerned that the native grasses that
25 are proposed for that crosshatched
26 area will create edge habitat, which is

1 attractive to birds, rodents, small
2 animals, that type of thing that are
3 not consistent with safety at the
4 airport.

5
6 CHAIRMAN THEODORE: All right. Thank you.
7 I'd say, again, you know, that as far
8 as the LMO says that this is native
9 vegetation and I guess until that LMO
10 changes I think the native grasses
11 is -- I could see, you know, like I
12 said it already -

13
14 MR. PARKER: There's no
15 native grass that's a grass?

16
17 CHAIRMAN THEODORE: Well I think it's
18 gotten to a point where -

19
20 MR. PARKER: What did they
21 use at Palmetto Bluff that was
22 supposedly a native grass for their
23 golf course?

24
25 CHAIRMAN THEODORE: I think that he's
26 adding more to the fruit - it's the height of the material as well.

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MR. PARKER: Yeah.

CHAIRMAN THEODORE: -- it's the height of the material that will harbor-

MR. PARKER: Right.

CHAIRMAN THEODORE: -- wild life there.

MR. PARKER: Right. But there isn't a native grass that's a grass that's, you know, a grass?

MS. WELCH: Yeah.

MR. ROAN: Like what might roll off a sod truck?

MR. PARKER: I don't know. There's no native grass? None?

MR. ROAN: At Palmetto Bluff it is probably what's called paspalum -- you can water it with salt water.

MR. PARKER: Right, that's what I was trying to think of. But

1 it's not native?

2

3 MR. ROAN: No, it's

4 not native, it's actually a hybrid. It

5 was a laboratory invention.

6

7 CHAIRMAN THEODORE: Most of the grasses

8 are -

9

10 MR. ROAN: Most of

11 them -- yeah, most of them are.

12

13 CHAIRMAN THEODORE: But most of the native

14 grasses are kind of clumpers.

15

16 MR. ROAN: Yeah. I

17 think we recommended Muhly grass, which

18 is the stuff that gets real purple in

19 the fall, cord grass or Spartina grass

20 which if you go out to Shelter Cove

21 Park it's all the stuff on the bank and

22 then Broomsedge and it's just another

23 real wiry cord grass, those are the

24 three we recommended. Those are --

25 grow two to three feet tall they're

26 going to keep mowing them. I mean they're

1 always going to be eight inches tall, so
2 whatever you're putting out there's
3 going to be eight inch tall grass. I
4 would presume.

5
6 CHAIRMAN THEODORE: Would the applicant
7 mind coming back up again? Sorry,
8 having to get up and sit down quite a
9 bit.

10 I think where we are is do you get
11 a sense of kind of the recommendations
12 that we've laid out here that we would
13 recommend that you take this back and
14 look at the options? I believe that --
15 I think we're all in agreement that
16 this buffer along this corridor that
17 has that power easement or the sewer
18 easement that runs through it should be
19 explored with either being a full
20 75-foot depth of vegetative buffer or,
21 you know, if a berm can be worked into
22 it, that you explore what the
23 implications are of the berm versus
24 the vegetation. Our goal is to make sure
25 that it is a visual barrier there and
26 that we get the full 75 feet and you've

1 already recommended that you can take
2 that fire lane and vegetate that full
3 depth, as well, and then I guess in
4 doing so I guess you decide whether
5 you'd move that fence line 15 feet in
6 to keep the buffer on the outside of
7 the fence but however that relates
8 there and works with the FAA regulations.

9
10 MR. STEARNS: I believe I
11 understand what you're asking us to do.
12 We've got the appropriate number of
13 plants to mitigate what was removed and
14 that would mean that we remove some of
15 the plants that are -- where we more
16 sparsely plant beyond the 75-foot
17 buffer. We would move some of those
18 plants into the fire lane area that we
19 preserved, and if planting's required
20 along the, you know, an additional
21 15 feet along Beach City Road those
22 plants -- the remainder of those plants
23 would move there rather than to be
24 planted in that area that's pretty much
25 preserved now.

26

1 CHAIRMAN THEODORE: Well, I think there's
2 -- in addition to the mitigation of the
3 trees, there's an agreement that you
4 will establish a 75-foot buffer. You
5 know, what was on the table was if you
6 couldn't fill that buffer with trees
7 you would throw, you know, that cost
8 into a mitigation bank with the Town.
9 This may be the opposite of where you may
10 have to supplement to maintain that
11 75-foot vegetated buffer.

12
13 MR. ROAN: I'd like to
14 offer something, purely from a
15 mathematical standpoint, from this
16 point on he has a 75-foot buffer. He's
17 got a buffer, you know, all of this
18 counts for 75 feet where it
19 needs to be supplemented is from this
20 corner here.

21
22 CHAIRMAN THEODORE: Correct.

23
24 MR. ROAN: I'm going
25 to figure this computer out before I
26 leave --.

1 MR. PARKER: Along the
2 grassed area.

3

4 MR. ROAN: Along the
5 grassed area.

6

7 MR. PARKER: Yeah, yeah,
8 we understand that. We understand
9 that.

10

11 MS. LEWIS: I, too, in
12 that if PSD agrees to some planting
13 then it doesn't have to be trees.
14 Flexibility in the LMO says that we
15 have count that as the buffer.

16

17 CHAIRMAN THEODORE: Sure and
18 that's another option, as well.

19

20 MS. LEWIS: Of beyond that 75 feet.

21

22 CHAIRMAN THEODORE: I think we're all in
23 agreement that the goal is to try to
24 establish 75 feet of depth whether it's
25 over top of that easement area or if
26 it's, you know, widening the depth, you

1 know, either way is to -- I mean that's
2 where the sensitivity is really coming
3 into play is along that edge over
4 there, so.

5

6 MS. LEWIS: I just
7 didn't want anybody to think that the
8 only option is going an additional 15 feet.

9

10 CHAIRMAN THEODORE: Well, I think the
11 question is, you know, would they allow
12 this type of material in there.

13

14 MS. LEWIS: Before this
15 comes before you all again, I'll talk to
16 the PSD, it doesn't have to be
17 the -- material grass
18 and things like that but the roots
19 don't get to the height but it'd still
20 be able to count toward the vegetated buffer.

21

22 CHAIRMAN THEODORE: Yeah. I'm just not
23 sure the grasses will do the same
24 treatment in terms of what this
25 vegetation will do in terms of the
26 height. I mean, you know, we're

1 actually talking about the grasses on
2 the other side of this buffer.

3

4 MR. ROAN: If you're going to send them on an
5 errand to study berms, you've got to give them
6 a height. Give them a height. How
7 tall of a berm do you want them to look
8 at 4-feet 8, I mean it's -- there's -

9

10 CHAIRMAN THEODORE: I mean, I think for the most
11 part if there's a berm it's a visual
12 element, it's not -- I'm not a scientist
13 about the noise and stuff, but I'm not
14 sure how much it would do in terms of
15 abatement of airplane noise.

16

17 MR. PARKER: I think if
18 the berm can't about in the historic
19 strip of property we're talking about
20 and saving some of that potential
21 digging up historic artifacts and
22 beyond the roadside which is kind of
23 the same place I don't know that the
24 berm -- personally I'm not asking for a
25 berm.

26

1 MR. GARTNER: Nor am I.

2

3 CHAIRMAN THEODORE: How does anybody else
4 feel?

5

6 MR. SMITH: I don't want it.

7

8 MS. MOFFET: Well, I think the
9 historic aspect is a big one. Like I
10 said, I think the advisory opinion
11 would be, you know, just in terms of -

12

13 MR. PARKER: Could it be
14 along the road is the question?

15

16 MS. MOFFETT: Right, right.

17

18 CHAIRMAN THEODORE: The berm?

19

20 MR. PARKER: yes. I
21 don't know. I'm not an expert. I
22 don't know. I'd like him to look at
23 it.

24

25 MS. WELCH: Yeah.

26

1 MR. PARKER: Tell us
2 they can't -
3
4 MR. STEARNS: You mean on
5 the road right-of-way?
6
7 MR. PARKER: Between the
8 sewer and the road.
9
10 CHAIRMAN THEODORE: But what you'd be
11 saying - there are trees there and
12
13 MR. STEARNS: Yes, there are trees there.
14
15 MR. PARKER: Oh well, then it's a non-starter. I forgot
16 about that part. It's been a while since we talked about the trees,
17 but yeah.
18
19 CHAIRMAN THEODORE: I'm not sure that the
20 berm will create that Hilton Head
21 natural environment as well.
22
23 MR. PARKER: It can be
24 made that way but.
25
26 CHAIRMAN THEODORE: But then when

1 you push it right up on right-of-way

2 I'm not sure it will -

3

4 MR. PARKER: I agree.

5

6 MS. WELCH: The trees I

7 think are the real reason not to do it

8 there.

9

10 CHAIRMAN THEODORE: I think if you put it

11 behind the 75-foot line then it could

12 be more blending but it sounds like

13 that may not be -- you know, you're not

14 in favor of it if it's that far back,

15 so.

16

17 MR. PARKER: I'm not

18 sure it's doing anything for us at

19 that point.

20

21 CHAIRMAN THEODORE: Are we concluding that

22 they look at the ability to get a full

23 depth of vegetative buffer in there of

24 75 feet?

25

26 MR. SMITH: If the berm

1 is going to do anything it's going to
2 have to be probably a minimum of ten
3 feet high.

4

5 CHAIRMAN THEODORE: Yeah.

6

7 MR. SMITH: And even
8 higher than that. The further back it
9 is the higher it's going to have to be.

10

11 CHAIRMAN THEODORE: Right.

12

13 MS. MOFFETT: Then we get
14 into the air space issue.

15

16 MR. SMITH: I'm not in
17 favor of it. I think the planting is
18 consistent with Hilton Head natural
19 plantings as been designated here I
20 think that's the way we should go.

21

22 CHAIRMAN THEODORE: I'm in agreement with
23 you, as well.

24

25 MR. PARKER: Is that
26 clear as mud for you?

1 MR. STEARNS: Okay. And
2 the other thing I do want you to know
3 that we did, early on, communicate with
4 the PSD about the sewer easement and
5 the types of trees that we had selected
6 to be planted in the buffer area they
7 would not allow in there but Teri's
8 having some additional conversation
9 about maybe some shrub type of
10 materials I suppose but.

11
12 MR. ROAN: Let me just
13 for maybe Kathleen's sake as much as
14 anything else can we just do one last
15 laundry list of things we want to
16 explore for the record.

17
18 CHAIRMAN THEODORE: All right. Try to
19 summarize this.

20
21 MR. ROAN: And I am
22 going to have to read this at some
23 point so.

24
25 CHAIRMAN THEODORE: We've talked about the
26 fence needs to be the black vinyl clad.

1 The buffer along the perimeter needs to
2 be a full 75 feet so you can take some
3 of these other areas where you're
4 deeper, Mike had pointed out, move some
5 of that over in there, fill in that
6 fire lane and then along this corridor
7 that's shown up on the image here is
8 that I think we're in agreement that
9 don't do the berm but look at the best
10 way to get that full 75 feet of
11 vegetative buffer, whether you can do
12 something in that PSD easement or add
13 an extra 15 feet to the inside over
14 there as well and as we stand right now
15 in representing the LMO and the
16 aesthetics that this clearcut area
17 needs to be revegetated with native
18 material and I think we all agree with
19 that rather than the ornamental grass,
20 which is the Bermuda grass and until
21 some other agreement changes that we
22 would like to see a solution in that
23 area.

24 And then in terms of maintenance I
25 would highly recommend that there be
26 temporary irrigation put in by the

1 contractor when he's installing the
2 plant material and that that system is
3 able to stay in place even beyond that
4 first year and that there be some sort
5 of maintenance agreement that will
6 extend for at least three more years
7 beyond that point and that this
8 irrigation system has the ability to
9 stay in place to continue to supplement
10 the material until it's fully
11 established if it takes more than a
12 year but maintenance of this is an
13 extremely important area as we all
14 agree that this will set precedence for
15 other areas in the future so this is
16 just beginning and it'd be good to
17 begin to budget and identify a
18 maintenance program for these types of
19 areas to make sure that they are
20 established and that they grow in
21 properly.

22 And then based on staff's comments
23 that when the initial installation is
24 done that there'll be a field
25 observation, look for those weak visual
26 points where you can see through the

1 buffer and that those will be
2 supplemented, as well, as part of that
3 process. I don't know if I've missed
4 anything.

5
6 MR. GARTNER: If we could
7 just follow up, also, on the Little
8 Gems. Just to -- I don't -- maybe
9 there isn't another option but maybe
10 there is.

11
12 CHAIRMAN THEODORE: They could look into
13 alternatives to the Little Gem. I
14 don't know, were they -- are there
15 alternatives to the Little Gem that's
16 on the list, Mike?

17
18 MR. ROAN: I don't recall from memory.

19
20
21 MR. GARTNER: Maybe still
22 in that same Magnolia family but just

23
24 CHAIRMAN THEODORE: there are some other hybrids of
25 Magnolia that fit the Little Gem. I
26 don't know if that forces you come up

1 with an alternative I mean to amend the
2 LMO but if that's in the LMO that
3 there's some other options for the -- a
4 broad leafed evergreen like that.

5
6 MR. STEARNS: Yeah. We
7 looked at options and there may be
8 others, I'll get -- when I get back
9 I'll get with my landscape architect
10 and see if there's another selection
11 that meets the height requirements and
12 that would also be we like the fact
13 this is an evergreen so it's green
14 pretty much year round.

15
16 CHAIRMAN THEODORE: Evergreen in a
17 broad leaf so it gives some variation
18 out there so it has some texture to
19 that buffer as well that not all the
20 vegetation has the real fine leaf type
21 character to it, so. Any other -

22
23 CHAIRMAN THEODORE: -- comments?

24
25 CHAIRMAN THEODORE: I might
26 add, too, that we meet every two weeks

1 so we'd be more than happy to -

2

3 MR. ROAN: If you can
4 get this turned around before the next
5 meeting we can put this on the next agenda.

6

7 CHAIRMAN THEODORE: Because we
8 would like help move it along, too.

9 All right. Thank you.

10 All right. Well this concludes our
11 meeting. Thank you.

12

13 The meeting was adjourned at approximately 3:40p.m.

14

15 Prepared by:

16

Kathleen A. Carlin
Secretary to the Design Review Board

17

18

19

20

21

22 Certified by:

23

Cori Brock

24

25 Town Clerk - Town of Hilton Head Island

26

27



Town of Hilton Head Island
 Community Development Department
 One Town Center Court
 Hilton Head Island, SC 29928
 Phone: 843-341-4757 Fax: 843-842-8908
www.hiltonheadislandsc.gov

FOR OFFICIAL USE ONLY	
Date Received:	7/11/12
Accepted by:	DA
App. #: DR	120021
Meeting Date:	

Applicant/Agent Name: Bob Frauhiger Company: Palmetto Bay Marina
 Mailing Address: 86 Helmsman Way City: HHI State: SC Zip: 29928
 Telephone: 785-7397 Fax: _____ E-mail: PPINC98@yahoo.com
 Project Name: Palmetto Bay Marina Project Address: same
 Parcel Number [PIN]: R 314A 272
 Zoning District: _____ Overlay District(s): _____

DESIGN REVIEW BOARD (DR) SUBMITTAL REQUIREMENTS

Digital Submissions may be accepted via e-mail by calling 843-341-4757.

Project Category:

_____ New Development – Conceptual _____ Alteration/Addition
 _____ New Development – Final, indicate Project Number _____ Minor External Change

Submittal Requirements for *All* projects:

_____ Architectural Review Board (ARB) Notice of Action (if applicable): When a project is within the jurisdiction of an ARB, the applicant shall submit such ARB's written notice of action per LMO Section 16-3-1004. Submitting an application to the ARB to meet this requirement is the responsibility of the applicant.

_____ Filing Fee, New Development \$175, Alterations/Additions \$100, Minor External \$50 cash or check made payable to the Town of Hilton Head Island.

Additional Submittal Requirements:

New Development – Conceptual Approval

- _____ A survey (1"=30' minimum scale) of property lines, existing topography and the location of trees meeting the tree protection regulations of Sec. 16-3-405, and if applicable, location of bordering streets, marshes and beaches.
- _____ A site analysis study to include specimen trees, access, significant topography, wetlands, buffers, setbacks, views, orientation and other site features that may influence design.
- _____ A draft written narrative describing the design intent of the project, its goals and objectives and how it reflects the site analysis results.
- _____ Context photographs of neighboring uses and architectural styles.
- _____ Conceptual site plan (to scale) showing proposed location of new structures, parking areas and landscaping.
- _____ Conceptual sketches of primary exterior elevations showing architectural character of the proposed development, materials, colors, shadow lines and landscaping.

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New Development – Final Approval

- _____ A final written narrative describing how the project conforms with the conceptual approval and design review guidelines of Sec. 16-4-503.
- _____ Final site development plan meeting the requirements of Sec. 16-3-303.F.
- _____ Final site lighting and landscaping plans meeting the requirements of Sec. 16-3-304 and Sec. 16-3-305.
- _____ Final floor plans and elevation drawings (1/8"=1'-0" minimum scale) showing exterior building materials and colors with architectural sections and details to adequately describe the project.
- _____ A color board (11"x17" maximum) containing actual color samples of all exterior finishes, keyed to the elevations, and indicating the manufacturer's name and color designation.
- _____ Any additional information requested by the Design Review Board at the time of concept approval, such as scale model or color renderings, that the Board finds necessary in order to act on a final application.

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- ✓ _____ Drawings of the proposed development - 11"x 17" .
- _____ Material/color samples of existing and proposed changes - 8 1/2" X 14" *Maximum*;
Stating manufacturer and material name

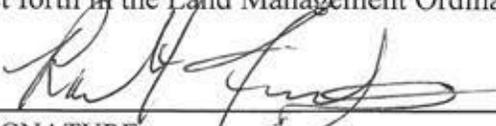
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Are there recorded private covenants and/or restrictions that are contrary to, conflict with, or prohibit the proposed request? If yes, a copy of the private covenants and/or restrictions must be submitted with this application. YES NO

To the best of my knowledge, the information on this application and all additional documentation is true, factual, and complete. I hereby agree to abide by all conditions of any approvals granted by the Town of Hilton Head Island. I understand that such conditions shall apply to the subject property only and are a right or obligation transferable by sale.

I further understand that in the event of a State of Emergency due to a Disaster, the review and approval times set forth in the Land Management Ordinance may be suspended.



SIGNATURE

 Jun 10, 2012
DATE



July 10, 2012

Town of Hilton Head Island
Design Review Board
One Town Center Court
Hilton Head Island, SC 29928

The retail buildings at Palmetto Bay Marina were built in 1982 and the outside wall surfaces are tabby with a light beige, white and gray color. Most of the shells are gone from the numerous powers washing over the past 30 years.

We would like to repair and paint the lower walls under the covered walkways, awnings and patio areas using a light gray elastameric paint ICI Decra Flex by Glidden.

We think the light gray color will be a pleasant contrast with the white rails and columns in the front of the buildings and the red Metal roof we installed in 2009.

Respectfully
Bob Frauhiger
Palmetto Bay Marina

SW 6245
Quicksilver

X

SW 6246
North Star

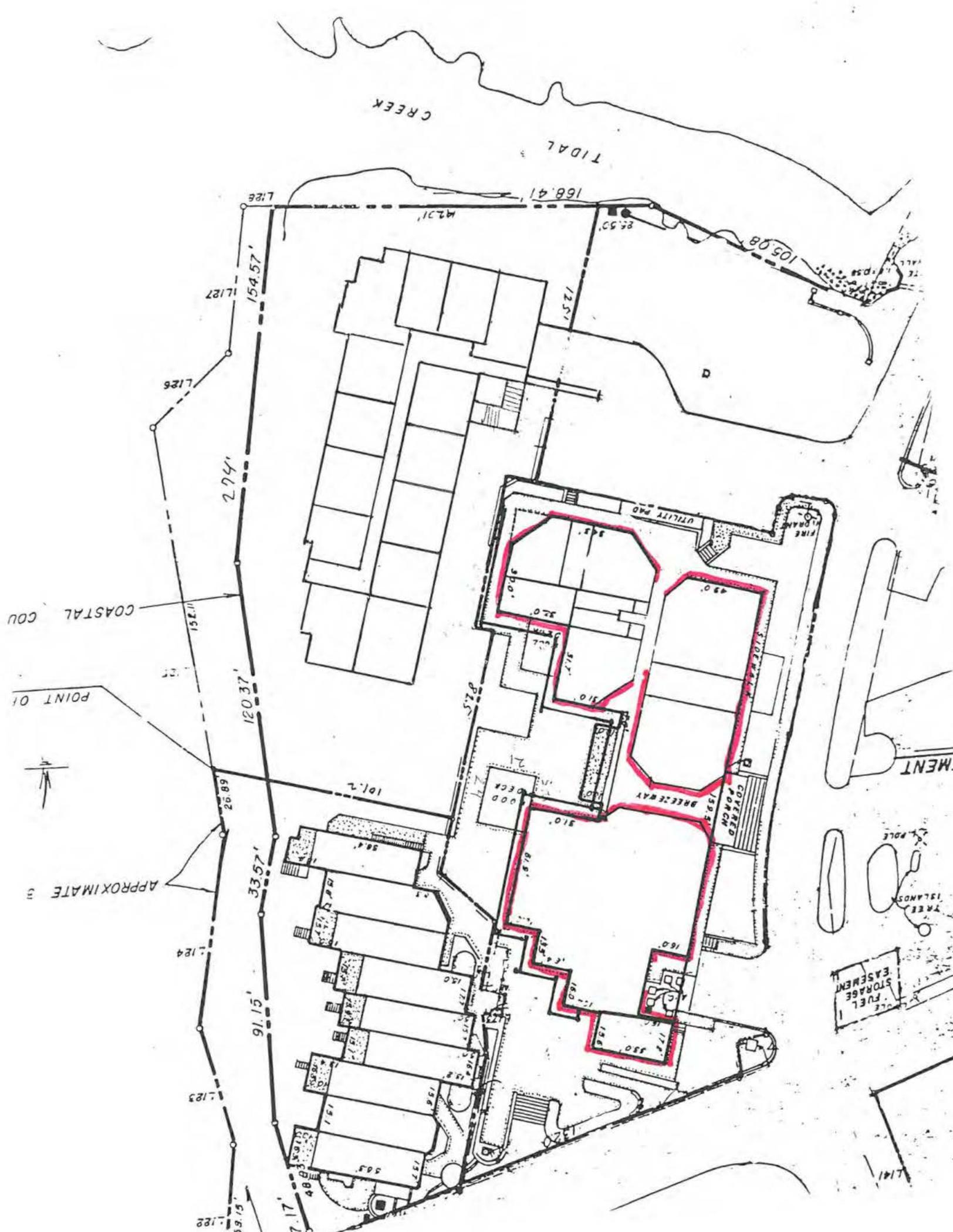
SW 6247
Krypton

SW 6248
Jubilee

SW 6249
Storm Cloud

SW 6250
Granite Peak

SW 6251
Outerspace



The BLACK MARLIN
BAYSIDE GRILL

Captain Woody's

Capt Woody's
SEAFOOD & BISTRO









712

NO PARKING FIRE LANE







K

SW 7063
Nebulous White

SW 7064
Passive

SW 7065
Argos



36

SW 6245
Quicksilver

SW 6246
North Star

SW 6247
Krypton

DESIGN TEAM/DRB COMMENT SHEET

*The comments below are staff recommendations to the Design Review Board (DRB)
and do NOT constitute DRB approval or denial.*

PROJECT NAME: Palmetto Bay Marina – MINOR EXTERNAL CHANGE

DRB#: DR 120021

DATE: August 14, 2012

RECOMMENDATION: Approval Approval with Conditions Denial

Based on the existing building colors/materials as well as the colors of surrounding buildings (brown, tan, beige, etc), staff recommends a warm light gray (SW 7064) versus a cool light grey (SW 6246) be used to repaint the lower walls of the buildings.



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 Community Development Department
 One Town Center Court
 Hilton Head Island, SC 29928
 Phone: 843-341-4757 Fax: 843-842-8908
www.hiltonheadislandsc.gov

FOR OFFICIAL USE ONLY
 Date Received: 7/30/12
 Accepted by: DH
 App. #: DR 120022
 Meeting Date: _____

Applicant/Agent Name: Collins Hartog Company: HARTZOG HOLDINGS
 Mailing Address: 109 Still Wild Lane City: Elgin State: SC Zip: 29045
 Telephone: 803 361-6718 Fax: _____ E-mail: collins@hartzogholdings.com
 Project Name: Advance AWD - Matthews Drive Project Address: 120 Matthews Drive
 Parcel Number [PIN]: R511 008 000 0154 0000
 Zoning District: CC Overlay District(s): Coastal

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- All sent electronically to Jennifer Ray.*

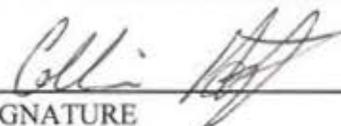
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SIGNATURE

7-30-12

DATE

Narrative for 120 Matthews Drive Advance Auto Project

The property is located at 120 Matthews Drive and 7 Shrimp Lane on Hilton Head Island. The property was put under contract with the intent of opening an Advance Auto store. Advance Auto is the nation's second largest auto parts dealer and has been assessing the Hilton Head market for several years. Several pieces of property were evaluated prior to committing to this parcel. Although there were several complications with this property, it was the best option for a variety of reasons including location and size of current structure on the property. The first obstacle was having the property rezoned to allow both commercial and retail sales. This was successfully accomplished earlier this year. The second obstacle is having the rear building removed to accommodate additional parking and also improving the front building so a portion can be used as retail sales for the general public. Advance Auto wants to have a convenient and appealing aesthetic that meets all the municipal requirements while allowing them to be competitive in the auto parts business. The current building is structurally sound and the foot print allows for an approved floor plan by Advance Auto. It is imperative to keep the existing foot print of the building while improving the front portions appearance. As the elevations and site layout will indicate we are intending to use the front of the building for a limited retail use while using the rear for commercial accounts and to warehouse parts. The current landscaping will be kept with some small additions. Shrimp Lane will be improved with the consent of the neighboring businesses and owners. The costs associated with the purchase of the property and the improvements are critical to the long term success of the business. One main reason Advance Auto is locating on Matthews Drive as opposed to Hwy 278 is the higher land costs and lack of existing structures suitable for their store layout. We hope to gain approval from the DRB, DPR, and receive permits in time to open this store in the fall of 2012. Thank you for your consideration and time.



WARNER ARCHITECTURAL ASSOCIATES

109 Anderson St. Suite 105
Marietta, GA 30060

Phone: 678-290-9200
Fax: 678-290-9250
E-mail: info@WarnerArchAssoc.com

July 30,2012

Warner Architectural Associates
109 Anderson Street Suite 105
Marietta, GA 30060

Re: Advanced Auto Parts
120 Matthews Drive & Shrimp Ln
Hilton Head, SC

TO WHOM IT MAY CONCERN:

The following is a brief description of the proposed Advance Auto Parts located at 120 Matthews Dr & Shrimp Ln, Hilton Head, SC.

The building is an existing commercially zoned metal building. In efforts to satisfy “Island Character” we have proposed the exterior of our building design to have a subtle visual impact and have chosen to utilize natural materials, textures and colors. Also we have provided continuity of design on the facades of the building. Incorporated in our design materials will be wood, efis and existing metal panels. All materials are designed to meet “Island Character.”

If you have any questions or concerns please do not hesitate to contact me.

Sincerely,

Ryan Warner
Warner Architectural Associates
109 Anderson Street, Suite 109
Marietta, GA 30060



4006 Barrett Drive, Suite 203
Raleigh, North Carolina, 27609
TEL (919) 553-6570

Design Intent Narrative

The current parcel contains two existing buildings. The smaller building will be demolished and replaced with new parking area. The proposed parking area will meet the required number per the Town's Ordinance. New landscaping around parking and perimeter per Town's Landscape Ordinance will be installed. The larger building will be renovated to accommodate an Advance Auto Parts store.

The impervious surface change will reflect a net decrease in impervious area on the site by 4,258 SF (0.10 Acre).



7 Shrimp Lane Building to be Demolished



120 Matthews Drive View of Front of Building



120 Matthews Drive View of Rear of Building



Retail Across Matthews Drive from Building



Retail Across Shrimp Lane from Building



120 Matthews Drive Side View of Existing Building



120 Matthews Drive View of Entire Building

First American Title Insurance Company
 SCHEDULE B - SECTION II
 EXCEPTIONS
 Issuing Office File No: 12-04-029
 Any policy we issue will have the following exceptions unless they are taken care of to our satisfaction.

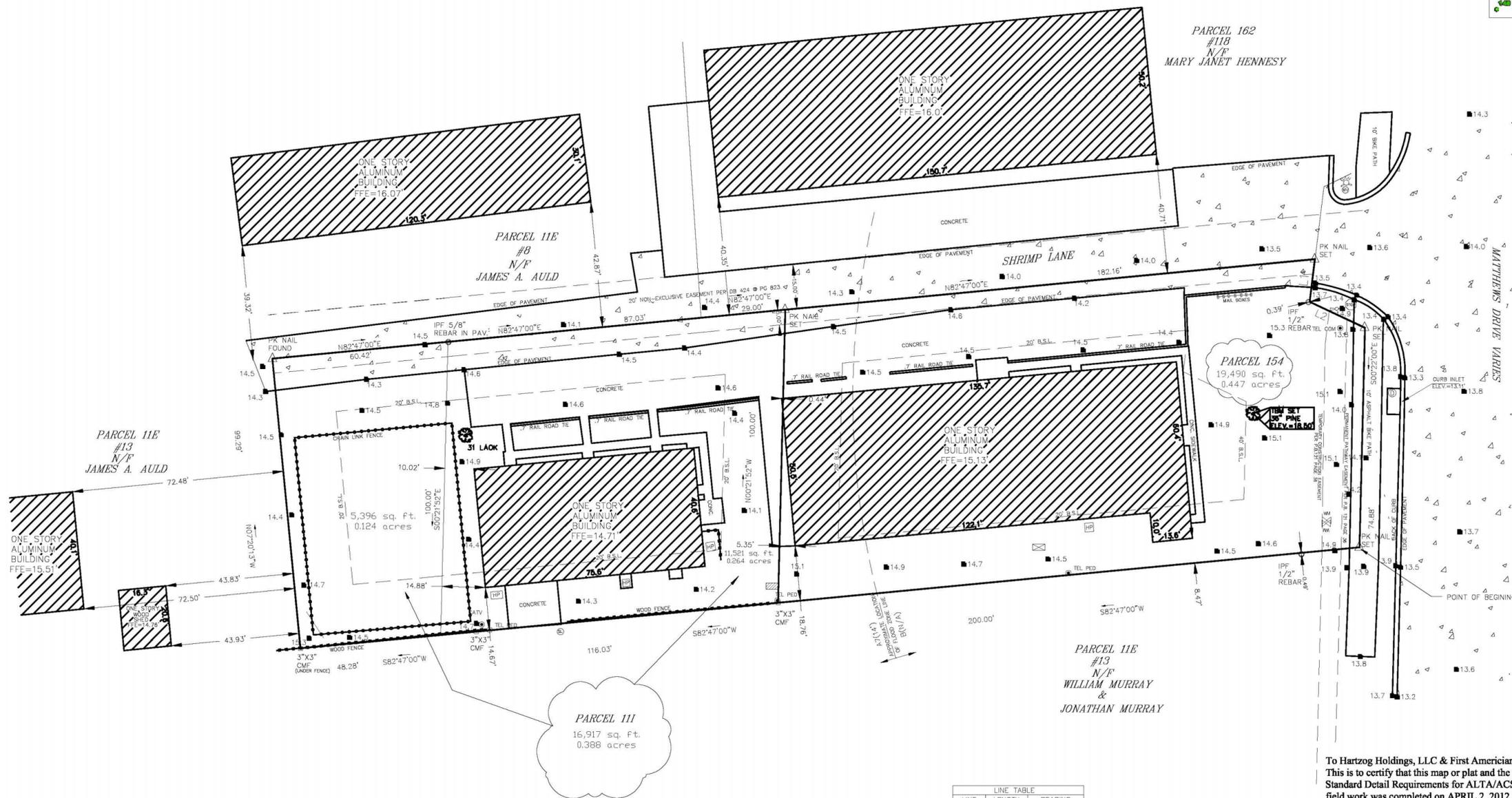
- Terms and conditions of any existing unrecorded lease(s) and all rights of lease(s) and any parties claiming through the lease(s) under said lease(s).
- Agreement by and between Adrienne K. and William F. Marscher, John L. Allen and James Auld, as recorded in Book 424 at Page 823 (PLATTED), Office of the Register of Deeds for Beaufort County.
- Easements to James A. Auld as recorded in Book 379 at Page 621 (VOIDED PER QUIT CLAIM DEED 424 AT 823), Book 424 at Page 836 (PLATTED) and in Book 424 at Page 838 (PLATTED), Office of the Register of Deeds for Beaufort County.
- Quit Claim Deed to any and all easements or rights of ingress and egress as recorded in Book 424 at Page 817, Office of the Register of Deeds for Beaufort County.
- Easement agreement between John L. Allen and William F. and Adrienne K. Marscher as recorded in Book 424 at Page 832 (PLATTED), Office of the Register of Deeds for Beaufort County.
- Easement to Palmetto Electric Cooperative, Inc. as recorded in Book 2400 at Page 2094 (UNDERGROUND UTILITY NOT PLATTABLE), Office of the Register of Deeds for Beaufort County.
- Easement Agreement between William F. Marscher II, Trustee and Frances H. Marscher, Trustee of the Marscher II Trust and the Town of Hilton Head, as recorded in Book 424 at Page 832 (PLATTED), Office of the Register of Deeds for Beaufort County. NOTE: This matter applies to 120 Mathews Drive only
- Possible rights of Rose Supply Corporation under certain Lease Agreement recorded in Book LD10 at Page 138, Office of the Register of Deeds for Beaufort County. NOTE: This matter applies to 120 Mathews Drive only.
- Covenant by James A. Auld regarding construction of warehouses as recorded in Book 383 at Page 1786, Office of the Register of Deeds for Beaufort County. NOTE: This matter applies to 7 Shrimp Lane only.

PROPERTY DESCRIPTION
 ALL THAT CERTAIN PIECE, PARCEL OR LOT OF LAND LYING AND BEING IN THE TOWN OF HILTON HEAD ISLAND, BEAUFORT COUNTY, STATE OF SOUTH CAROLINA, BEING LOCATED ON THE WESTERN RIGHT OF WAY OF MATTHEWS DRIVE, KNOWN AS PARCELS 154 AND 111 AND HAVING THE FOLLOWING METES AND BOUNDS TO WIT:
 BEGINNING AT A PK NAIL SET ON THE WESTERN RIGHT OF WAY OF MATTHEWS DRIVE, AND NOTED AS POINT OF BEGINNING, BEING THE COMMON CORNER OF PARCELS 154 AND 111; THENCE, RUNNING IN A WESTERLY DIRECTION S82°47'33"W FOR A DISTANCE OF 200' TO A 3" x 3" CONCRETE MONUMENT; THENCE, S83°47'00"W FOR A DISTANCE OF 116.03' TO A POINT; THENCE, S82°47'00"E FOR A DISTANCE OF 48.28' TO A 3" x 3" CONCRETE MONUMENT; THENCE, N07°20'13"W FOR A DISTANCE OF 99.29' TO A PK NAIL FOUND; THENCE, N82°47'00"E FOR A DISTANCE OF 60.42' TO A 5/8" REBAR FOUND; THENCE, N82°47'00"E FOR A DISTANCE OF 87.03' TO A POINT; THENCE, N82°47'00"E FOR A DISTANCE OF 29.00' TO A PK NAIL SET; THENCE, N82°47'00"E FOR A DISTANCE OF 182.16' TO A PK NAIL SET; THENCE, S05°17'19"W FOR A DISTANCE OF 14.65' TO A 1/2" REBAR; THENCE, S66°38'43"E FOR A DISTANCE OF 20.92' TO A PK NAIL SET; THENCE, S00°22'00"E FOR A DISTANCE OF 74.88' TO A PK NAIL SET BEING THE POINT AND PLACE OF BEGINNING AND CONTAINING 0.785 ACRES. THE PROPERTY DESCRIBED IS SUBJECT TO A 20' NON EXCLUSIVE EASEMENT ON THE NORTHERN MOST PROPERTY LINE, AND A PERMANENT PATHWAY EASEMENT ON THE EASTERN MOST PROPERTY LINE.



- LEGEND
- CMF - CONCRETE MONUMENT SET
 - IPF - IRON PIN FOUND
 - IPF - IRON PIN FOUND
 - IPF - IRON PIN FOUND
 - # - INDICATES STREET ADDRESS
 - TBM - TEMPORARY BENCH MARK
 - B.S.L. - BUILDING SETBACK LINE
 - TEL - TELEPHONE PEDESTAL / COMMUNICATORY / CATV
 - SL - SEWER LATERAL
 - SM - SANITARY SEWER MANHOLE
 - EB - ELECTRIC BOX
 - SES - SPOT ELEVATION SHOTS
 - TFM - TRANSFORMER
 - WV - WATER VALVE
 - WM - WATER METER
 - HP - HEAT PUMP
 - FH - FIRE HYDRANT
 - GI - GRATE INLET
 - PP - POWER POLE
 - D.H.P.L. - OVER HEAD POWER LINE
 - GL - GUY LINE
 - LP - LIGHT POLE
 - SD - STORM DRAIN MANHOLE
 - FM - FIBEROPTIC MANHOLE
 - SS - STREET SIGN
 - SS - STOP SIGN

- TREE LEGEND
- WOK - WHITE OAK
 - LOK - LAUREL OAK
 - LOK - LIVE OAK
 - WOK - WATER OAK
 - ROK - RED OAK
 - PCAN - PECAN
 - MAG - MAGNOLIA
 - HIC - HICKORY
 - MAP - MAPLE
 - PLM - PALMETTO
 - CHY - CHERRY
 - HLI - HOLLY
 - CDR - CEDAR
 - RDB - RED BUD
 - SAS - SASSAFRAS
 - DOG - DOGWOOD
 - SB - SUGARBERRY
 - P - PINE
 - G - GUM
 - B - BAY



LINE	LENGTH	BEARING
L1	14.65	S05°17'19"W
L2	20.92	S66°38'43"E

NOTE: PARCELS 111 & 154 ARE ZONED IL - LIGHT INDUSTRIAL PER THE TOWN OF HILTON HEAD PLANNING DEPARTMENT & THE SETBACKS SHOWN ARE PER THE TOWN OF HILTON HEAD PLANNING DEPARTMENT.

To Hartzog Holdings, LLC & First American Title Insurance Company:
 This is to certify that this map or plat and the survey on which it is based were made in accordance with the 2011 Minimum Standard Detail Requirements for ALTA/ACSM Land Title Surveys, jointly established and adopted by ALTA and NSPS. The field work was completed on APRIL 2, 2012.
 Date of plat: APRIL 4, 2012

WILLIAM J. SMITH, PLS # 26960



T-SQUARE GROUP, INC.
 PROFESSIONAL LAND SURVEYORS
 P.O. Drawer 330
 139 Burnt Church Road
 Bluffton, S.C. 29910
 tsquare@hargray.com
 Phone 843-757-2650 Fax 843-757-5758
 JOB No. 12-042ALTA



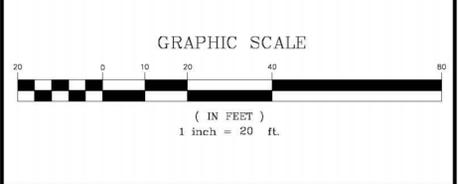
I HEREBY STATE TO THE BEST OF MY KNOWLEDGE, INFORMATION & BELIEF, THE SURVEY SHOWN HEREON WAS MADE IN ACCORDANCE WITH THE REQUIREMENTS OF THE MINIMUM STANDARDS MANUAL FOR THE PRACTICE OF LAND SURVEYING IN SOUTH CAROLINA, AND MEETS OR EXCEEDS THE REQUIREMENTS FOR A CLASS A SURVEY AS SPECIFIED THEREIN.
 ALSO THERE ARE NO VISIBLE ENCROACHMENTS OR PROJECTIONS AFFECTING THE PROPERTY OTHER THAN THOSE INDICATED.

WILLIAM J. SMITH, PLS # 26960

THE ABOVE PLAT PREPARED BY ME AT THE REQUEST OF
HARTZOG HOLDINGS, LLC
 AN ALTA/ACSM LAND TITLE SURVEY OF PARCELS 111 & 154 MATHEWS DRIVE, TOWN OF HILTON HEAD ISLAND, BEAUFORT COUNTY, SOUTH CAROLINA.

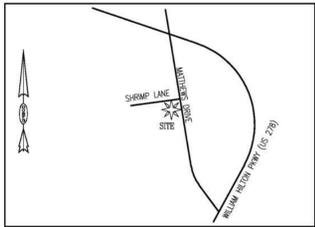
DIST 511, MAP 008, PARCEL 111
 DIST 511, MAP 008, PARCEL 154

Notes:
 1. According to FEMA Flood Insurance Rate Map # 450025 0000D This Lot Appears To Lie In A Federal Flood Plain Zone A7 & B, Minimum Required Elevation 14'0" & N/A, NGVD29
 2. This Survey Was Performed Without The Benefit Of A Wetland Delineation
 3. All Building Setback Requirements Should Be Verified With The Proper Authorities Prior To Design And Construction
 Reference: PLAT 163
 1. PLAT BOOK 33 AT PAGE 57
 2. PLAT BOOK 121 AT PAGE 36
 3. PLAT BOOK 23 AT PAGE 25
 4. A PLAT BY SEA ISLAND ENGINEERING, PREPARED FOR WILLIAM F. & ADRIENNE K. MARCHER.



DRAWN BY: W.J.S.
 APPROVED BY: F.F.B.
 PARTY CHIEF: B.S.
 DATE: APRIL 4, 2012

Notes:
 1. According To FEMA Flood Insurance Rate Map # 450025 0008D
 This Lot Appears To Lie In A Federal Flood Plain Zone A7 & B. Minimum
 Required Elevation 14.0' & N/A. NGVD29



VICINITY MAP
 NOT TO SCALE

DEVELOPMENT DATA:
 ZONING DISTRICT(S): COMMERCIAL CENTER
 MAXIMUM IMPERVIOUS AREA: 65%

GENERAL NOTES:

- THE CONTRACTOR SHALL VERIFY ALL NECESSARY BARRICADES, SIGNS, FENCES, FLASHING LIGHTS, FLAGMEN, ETC. FOR MAINTENANCE AND PROTECTION OF TRAFFIC AS REQUIRED BY TOWN OF HILTON HEAD AND THE SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION. REFER TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), LATEST EDITION, FOR DETAILS OF TRAFFIC CONTROL STANDARDS AND DEVICES.
- THE CONTRACTOR SHALL PROTECT ALL MONUMENTS, IRON PINS, AND PROPERTY CORNERS DURING CONSTRUCTION.
- APPROVAL OF THESE PLANS IS NOT AN AUTHORIZATION TO GRADE ADJACENT PROPERTIES. WHEN FIELD CONDITIONS WARRANT OFF-SITE GRADING, PERMISSION MUST BE OBTAINED FROM THE AFFECTED PROPERTY OWNERS.
- COORDINATE ALL CURB AND STREET GRADIES IN INTERSECTIONS WITH INSPECTOR.
- NON-STANDARD ITEMS (I.E. PAVERS, IRRIGATION SYSTEMS, ETC.) IN THE RIGHT-OF-WAY REQUIRE A RIGHT-OF-WAY ENFORCEMENT AGREEMENT WITH THE SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION BEFORE INSTALLATION.
- ALL SITE DIMENSIONS ARE REFERENCED TO THE FACE OF CURBS OR EDGE OF PAVING UNLESS OTHERWISE NOTED. ALL BUILDING DIMENSIONS ARE REFERENCED TO THE OUTSIDE FACE OF THE STRUCTURE.
- EXISTING SITE CONDITIONS, GRADIES, CONTOURS, UTILITIES AND OTHER EXISTING FEATURES FROM A SURVEY PREPARED BY T-SQUARE GROUP, INC.
- ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE LATEST EDITION OF THE SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION "STANDARD DETAILS AND SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" AND APPLICABLE TOWN OF HILTON HEAD CODES AND ORDINANCES. THE CONTRACTOR SHALL MAINTAIN A CURRENT EDITION OF THE STATE AND LOCAL CODES, ORDINANCES, STANDARD SPECIFICATION AND STANDARD DETAILS ON THE PROJECT SITE FOR REFERENCE DURING CONSTRUCTION OF THE PROJECT.
- THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS IN THE FIELD PRIOR TO THE START OF CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL FIELD DIMENSING AND SHALL REPORT ANY DISCREPANCIES BETWEEN THE PLANS AND ACTUAL FIELD CONDITIONS TO THE OWNER OR OWNER'S REPRESENTATIVE IMMEDIATELY. CONTRACTOR SHALL WAIT FOR INSTRUCTION PRIOR TO PROCEEDING WITH WORK.
- THE CONTRACTOR SHALL PROVIDE SMOOTH TRANSITIONS FROM PROPOSED FEATURES TO EXISTING FEATURES AS NECESSARY.
- THE CONTRACTOR SHALL SEAL THE EDGE OF EXISTING ASPHALT PAVEMENT WITH TACK COAT IN ACCORDANCE WITH THE SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS WHERE NEW PAVEMENT JOINS EXISTING PAVEMENT.
- THE CONTRACTOR SHALL REPAIR, RESURFACE, RECONSTRUCT OR REFURBISH ANY AREAS DAMAGED DURING CONSTRUCTION BY THE CONTRACTOR, HIS SUBCONTRACTORS OR SUPPLIERS AT NO ADDITIONAL COST TO THE OWNER.
- ALL PAVEMENT JOINTS SHALL BE SAWCUT PRIOR TO PAVING TO PROVIDE A DURABLE AND UNIFORM JOINT.
- CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF TRUCK DOCKS, EXIT DOORS, SIDEWALKS, PRECISE BUILDING DIMENSIONS, AND EXACT BUILDING UTILITY ENTRANCE LOCATIONS.
- ALL PAINT STRIPING, PAVEMENT MARKINGS, AND SIGNAGE SHALL CONFORM TO THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" OR AS OTHERWISE SPECIFIED. ALL REFERENCED SIGN STANDARDS ARE TAKEN FROM THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES". ALL NEW SIGNS SHALL BE MOUNTED ON GALVANIZED POSTS AND IN ACCORDANCE WITH STATE AND LOCAL REGULATIONS.
- CONTRACTOR SHALL INSTALL ACCESSIBLE RAMPS PER LOCAL MUNICIPALITY AND ADA STANDARDS AT ALL DRIVE AND BUILDING LOCATIONS AS REQUIRED.
- THE CONTRACTOR IS RESPONSIBLE FOR THE LOCATION AND PROTECTION OF ALL EXISTING UTILITIES DURING CONSTRUCTION. AT LEAST 48 HOURS PRIOR TO ANY DEMOLITION, GRADING OR CONSTRUCTION ACTIVITY THE CONTRACTOR SHALL NOTIFY THE TOWN OF HILTON HEAD PUBLIC UTILITIES DEPARTMENTS FOR PROPER IDENTIFICATION OF EXISTING UTILITIES WITHIN THE PROJECT SITE.
- PROOF ROLL BUILDING AND ALL PARKING AREAS. NOTIFY OWNER/ADVANCE AUTO PARTS OF ANY UNACCEPTABLE AREAS.
- THE CONTRACTOR SHALL PROVIDE AS-BUILT RECORDS OF ALL CONSTRUCTION (INCLUDING UNDERGROUND UTILITIES AND STORM DRAINAGE) TO OWNER/ADVANCE AUTO PARTS PRIOR TO COMPLETION OF CONSTRUCTION.

SITE GENERAL NOTES:

- A "TOWING SIGN" SIGN MUST BE INSTALLED AS SOON AS POSSIBLE BEFORE CONSTRUCTION AND MUST BE REMOVED DURING THE WEEK OF STORE MERCHANDISING. REFER TO DETAIL 9/C2.
- THE CONTRACTOR IS TO STAKE THE LOCATION OF THE PYLON SIGN AND NOTIFY ADVANCE AUTO PARTS CONSTRUCTION PROJECT MANAGER FOR THE EARLIEST POSSIBLE INSTALLATION DATE.
- PARKING LOT GRADING SHALL NOT EXCEED SIX MAX. CROSS SLOPE.
- PARKING SPACES ARE TO BE 10' X 20' UNLESS OTHERWISE NOTED.
- ALL ABANDONED ENTRANCES/EXITS ARE TO BE REMOVED.
- THE DEVELOPER/CONTRACTOR IS RESPONSIBLE FOR SETUP AND COSTS OF ALL TEMPORARY UTILITY SERVICES (INCLUDING TEMPORARY POWER) UNTIL THE PROJECT IS TURNED OVER TO AND ACCEPTED BY ADVANCE AUTO PARTS.
- THE CONTRACTOR IS TO PRESSURE WASH ALL PAVED AREAS AND SIDEWALKS THE MORNING PRIOR TO STORE OPENING.
- THE CONTRACTOR SHALL FURNISH AND INSTALL A MAILBOX ON SITE. THE MAILBOX TYPE, LOCATION, AND INSTALLATION SHALL COMPLY WITH THE LOCAL U.S. POST OFFICE REQUIREMENTS. VERIFY REQUIREMENTS PRIOR TO PURCHASE AND ROUGH-IN.
- BASE OF PARKING LOT LIGHT POLES SHALL REMAIN UNPAINTED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING SIGNED AND SEALED CERTIFICATION THAT PARKING LOT MEETS REQUIREMENTS SET FORTH IN THE CONSTRUCTION DOCUMENTS.

LINE	LENGTH	BEARING
L1	14.65	S05°17'19"W
L2	20.92	S66°38'43"E

IMPERVIOUS AREA SUMMARY

TOTAL SITE AREA =	36,407 SF	(0.836 AC)
EXISTING IMPERVIOUS AREA:	10,046 SF	(0.231 AC)
EXISTING PAVEMENT =	12,921 SF	(0.297 AC)
TOTAL IMPERVIOUS AREA =	22,969 SF	(0.528 AC)
EXISTING OPEN SPACE =	13,438 SF	(0.308 AC)
PROPOSED BUILDING =	6,845 SF	
PROPOSED SIDEWALK =	840 SF	
PROPOSED PAVEMENT =	11,026 SF	
TOTAL PROPOSED IMPERVIOUS AREA =	18,711 SF	(0.43 AC)
IMPERVIOUS AREA PERCENTAGE =	51.4%	
PROPOSED OPEN SPACE =	17,696 SF	(0.406 AC)
(NET 4,258 SF (0.10 AC) DECREASE IN IMPERVIOUS AREA ON THE SITE)		

SITE ARCHITECTURAL KEY NOTES:

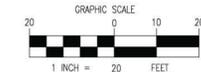
- 6" HIGH TREATED WOOD DUMPSTER SCREEN. PAINT TO MATCH THE BUILDING. REFER TO DETAILS SHEET C2.
- INSTALL NEW 6" THICK CONCRETE LOADING PAD OF 4000 PSI BROOM FINISHED CONCRETE WITH 6"x6"x 1/8" WWF STEEL REINFORCEMENT. SLOPE AWAY FROM BUILDING FOR POSITIVE DRAINAGE. REFER TO DETAIL 1/C2.
- ASPHALT PAVEMENT PARKING LINES 4" WIDE PAINTED WHITE WITH TWO (2) COATS OF SHERWIN WILLIAMS "HOTLINE" FAST DRYING TRAFFIC MARKING PAINT. HANDICAP PARKING & HANDICAP SYMBOLS TO BE PAINTED COLOR & SIZE AS REQUIRED BY THE GOVERNING ACCESSIBILITY CODE. CONCRETE PAVEMENT PARKING LINES FOR ACCESSIBLE SPACES SHALL BE THE SAME AS ABOVE EXCEPT USE BLUE OR YELLOW PAINT. REFER TO DETAILS 1/C3 AND 2/C3 FOR ADDITIONAL INFORMATION.
- CONSTRUCT 6" HIGH CONC. CURB & GUTTER CONTINUOUS AROUND SITE. BACKFILL TO TOP OF CURB. REFER TO DETAIL 3/C2. PROVIDE CATCH & SPILL WHERE APPLICABLE.
- ANCHOR DOWN CONCRETE WHEEL STOPS 42" FROM FACE OF BUILDING. REFER TO DETAIL 8/C2.
- ANCHOR DOWN CONCRETE WHEEL STOPS 30" FROM FACE OF CONCRETE WALK/SIDEWALK. REFER TO DETAIL 8/C2.
- PROVIDE HANDICAP RAMP WHEN PAVING IS NOT FLUSH WITH ENTRY SIDEWALK PER FEDERAL & LOCAL ACCESSIBILITY REQUIREMENTS. REFER TO DETAILS ON SHEET C3.
- CONCRETE APRON IS TO BE BROOM FINISHED 6" THICK WITH 6"x6"x1/8" WWF STEEL REINFORCEMENT. CONCRETE TO BE A 4000# MIX; DO NOT USE SLAG OR FLY ASH AS CEMENTITIOUS MATERIAL.
- HANDICAP PARKING SIGN FOR SPACES ADJACENT TO BUILDING ENTRANCE. REFER TO DETAIL 5/C3.
- NEW BROOM FINISHED CONCRETE SIDEWALK SLOPED AWAY FROM BUILDING 1/8"/FT. FOR POSITIVE DRAINAGE.
- ROAD PYLON SIGN FURNISHED & INSTALLED BY ADVANCE AUTO PARTS-- REFER TO SHEET S/L1 FOR ELECTRICAL REQUIREMENTS. VERIFY LOCATION WITH AMP AND SIGN INSTALLER. REFER TO DETAIL ON SHEET C3. --PYLON FOUNDATION, BOLTS AND PLATES TO BE INSTALLED BY G.C. PYLON SIGN FOUNDATION TO BE ENGINEERED TO BE SITE SPECIFIC AND FOR PYLON SIZE. --IF MONUMENT SIGN IS REQUIRED, GC TO CONSULT WITH ADVANCE AUTO PARTS' SIGN COORDINATOR AND ARCHITECTURAL DESIGNER FOR DETAILS. GC IS TO PROVIDE AND INSTALL MONUMENT BASE AND RELATED ELECTRICAL.
- ASPHALT PAVEMENT DESIGN AS PER GEOTECHNICAL REPORT SPECIFICATIONS.
 - STANDARD DUTY PAVEMENT
 - HEAVY DUTY PAVEMENT
 - POROUS ASPHALT PAVEMENT
- LANDSCAPING -- AREA OF NEW SOD.
- CONCRETE CURBS, REFER TO DETAIL 7/C2 FOR ADDITIONAL INFORMATION.
- PROVIDE BOLLARDS AT ALL EXTERIOR MOUNTED METERING DEVICES. (GAS, WATER, ELECTRIC) MIN. (2) TWO AS REQUIRED.
- SEEDED AREA
- SITE LIGHT
- PROPOSED PARKING COUNT

SITE DATA

SITE AREA	36,407 SQ. FT.	0.836 ACRE(S)	6,845 SF	0.16 ACRE(S)	18.8 % OF TOTAL AREA
BUILDING	11,866 SF	0.27 ACRE(S)	32.6 % OF TOTAL AREA		
PAVEMENT	18,711 SF	0.43 ACRE(S)	51.4 % OF TOTAL AREA		
TOTAL IMPERVIOUS AREA	17,696 SF	0.41 ACRE(S)	48.6 % OF TOTAL AREA		
GREEN/OPEN SPACE					
BUILDING SETBACKS					
	REQUIRED	PROVIDED			
FRONT	40 FT.	58 FT.			
REAR	20 FT.	170 FT.			
SIDE (NORTHERN PROPERTY LINE)	20 FT.	30 FT.			
SIDE (SOUTHERN PROPERTY LINE)	20 FT.	19(EXIST) FT.			
MAXIMUM BUILDING HEIGHT					
	35 FT				
PARKING PROVIDED					
	REQUIRED (15SP/4000SF GFA)	PROVIDED			
REGULAR	17 SPACES	20 SPACES			
HANDICAP	1 SPACES	2 SPACES			
TOTAL	18 SPACES	22 SPACES (120% OF REQUIRED)			(3 SPACES THAT ARE ABOVE 1.05% OF REQUIRED SPACES TO BE PEROUS PAVEMENT)

DRIVEWAY NOTES:

ALL DRIVEWAY PAVEMENT MARKINGS SHALL BE THERMOPLASTIC AND INSTALLED ACCORDING TO SCDOT STANDARDS AND SPECIFICATIONS.
 CONTRACTOR SHALL REFERENCE THE ARMS (ACCESS AND ROADSIDE MANAGEMENT STANDARDS) MANUAL FOR REQUIREMENTS AS NECESSARY.



COOPERATIVE WAY

MATTHEWS DRIVE VARIES

LANDSCAPING NOTES:

- CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLETING ALL REQUIRED LANDSCAPING AND IRRIGATION FOR THE ENTIRE SITE. TO INCLUDE BUT NOT LIMITED TO: SODDED AREAS, SHRUB BEDS, PARKING LOT ISLANDS, ROADSIDE SIGN BASE(S) AND MONUMENT PLANTERS.
- CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE CODES AND ORDINANCES REGARDING LANDSCAPING.
- IRRIGATION CONTRACTOR SHALL BE EXPERIENCED IN IRRIGATION DESIGN AND INSTALLATION AND SHALL PROVIDE PROOF OF CERTIFICATION AS A "CERTIFIED IRRIGATION CONTRACTOR" ACCORDING TO THE IRRIGATION ASSOCIATION OF AMERICA. CONTRACTOR SHALL PROVIDE AN IRRIGATION SYSTEM INSTALLATION WITH 100% COVERAGE OF DESIGNATED PLANTING AREAS USING HEAD TO HEAD COVERAGE MINIMIZING POSSIBLE OVERTHROW ONTO NON-POROUS SURFACES. IRRIGATION SYSTEM SHALL BE ZONED AND TIED AS APPROPRIATE TO MEET PLANT MATERIAL AND LAWN AREA WATERING REQUIREMENTS. TIMER/CONTROL TO BE LOCATED INSIDE BUILDING NEAR ELECTRICAL PANEL.
- IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ESTABLISH A HEALTHY STAND OF GRASS ON ALL SEEDED AREAS.
- IN THE EVENT THAT PLANTING BEDS AND MULCH ARE REQUIRED, THE CONTRACTOR SHALL INSTALL BLACK FABRIC WEED BLOCK LANDSCAPE MESH UNDER THE MULCH TO PREVENT WEED GROWTH.
- CONTRACTOR SHALL PROVIDE NATURAL TOPSOIL THAT IS FERTILE, FRABLE, WITHOUT MIXTURE OF SUBSOIL MATERIALS, AND OBTAINED FROM A WELL DRAINED, AVAILABLE SITE. IT SHALL NOT CONTAIN SUBSTANCES WHICH MAY BE HARMFUL TO PLANT GROWTH. TOPSOIL SHALL BE SCREENED AND FREE FROM CLUMPS, LIMBS, STONES, ROOTS, PLANTS, OR SIMILAR SUBSTANCES 1" OR MORE IN DIAMETER, DEBRIS, OR OTHER OBJECTS WHICH MIGHT BE A HAZARD TO PLANTING OPERATIONS. TOPSOIL SHALL CONTAIN AT LEAST 4-BX ORGANIC MATTER BY WEIGHT AND HAVE A PH RANGE OF 5.5 TO 7.0 OR AS APPLICABLE TO THE REGION.
- CONTRACTOR SHALL BE RESPONSIBLE FOR THE WATERING AND THE MAINTENANCE OF ALL LANDSCAPED AREAS UNTIL THE LATER OF: (a) THIRTY (30) DAYS FOLLOWING THE PLANTING OF THE GRASS AND SHRUBS, OR (b) THE DATE THAT ADVANCE AUTO PARTS OPENS FOR BUSINESS TO THE PUBLIC. CONTRACTOR TO WARRANT ALL LANDSCAPING FOR A TOTAL OF 1 YEAR.
- GENERAL CONTRACTOR IS TO CLEAN ENTIRE SITE OF ALL CONSTRUCTION DEBRIS AND RAKE ALL GRASS AREAS. GRASS (SOD) TO BE LEVEL, ROLLED AND MOVED.
- PROVIDE LANDSCAPE PLANS TO ADVANCE AUTO PARTS AND AS REQUIRED BY LOCAL JURISDICTION TO THE BLDG. DEPT. FOR REVIEW AND APPROVAL PRIOR TO START OF WORK.
- ALL LANDSCAPING, TREES, SHRUBS, ETC. SHALL NOT INTERFERE WITH THE VISIBILITY OF MAP MONUMENT SIGNAGE.
- CONTRACTOR TO VERIFY QUANTITIES PRIOR TO COMMENCING WORK.
- ALL PLANTING AREAS WITHIN PROPERTY BOUNDARY ARE TO BE WATERED WITH A FULL AUTOMATIC UNDERGROUND SPRINKLE SYSTEM WITH FRETZE GUARD. ALL IRRIGATION COMPONENTS SHALL BE CONTAINED WITHIN THE BOUNDARY OF THE SITE. IRRIGATION TO PLANTING AREAS OUTSIDE OF THE PROPERTY BOUNDARY SHALL ORIGINATE FROM IRRIGATION HEADS WITHIN THE PROPERTY BOUNDARY. CONTRACTOR SHALL RETAIN THE SERVICES OF A LICENSED IRRIGATOR WHO SHALL PROVIDE SCHEDULED IRRIGATION DRAWINGS WITH SUPPORTING PRESSURE LOSS AND FLOW CALCULATIONS. THESE SHALL BE SUBMITTED TO LANDSCAPE ARCHITECT AS SHOP DRAWINGS FOR REVIEW AND APPROVAL PRIOR TO COMMENCING WORK.
- OPEN AREAS WITH PLANTING BEDS SHALL BE MULCHED WITH HARDWOOD ONLY. PINE STRAW OR EQUIVALENT IS NOT AN ACCEPTABLE MULCH.

INDEX TO SHEETS

SHEET #	DESCRIPTION
C-1	SITE PLAN
C-2	SITE DETAILS
C-3	SITE DETAILS
C-4	DEMOLITION PLAN
C-5	GRADING, DRAINAGE, UTILITY & EROSION CONTROL PLAN
L-1	LANDSCAPE PLAN

Triangle Site Design, PLLC
 4006 Barrett Drive
 Suite 203
 Raleigh, NC 27609
 (919)550-6570

HARTZOG HOLDINGS, LLC
 1109 STILL WILD LANE
 FLEETWOOD, SC 29045
 803-361-6778
 collins@hartzogholdings.com

Advance Auto Parts
 100 MATTHEWS DRIVE
 TOWN OF HILTON HEAD
 BEAUFORT COUNTY, SC

DATE: 7/30/12
 JOB #: 001016
 DRAWN BY: 001016
 CHECK BY: 001016

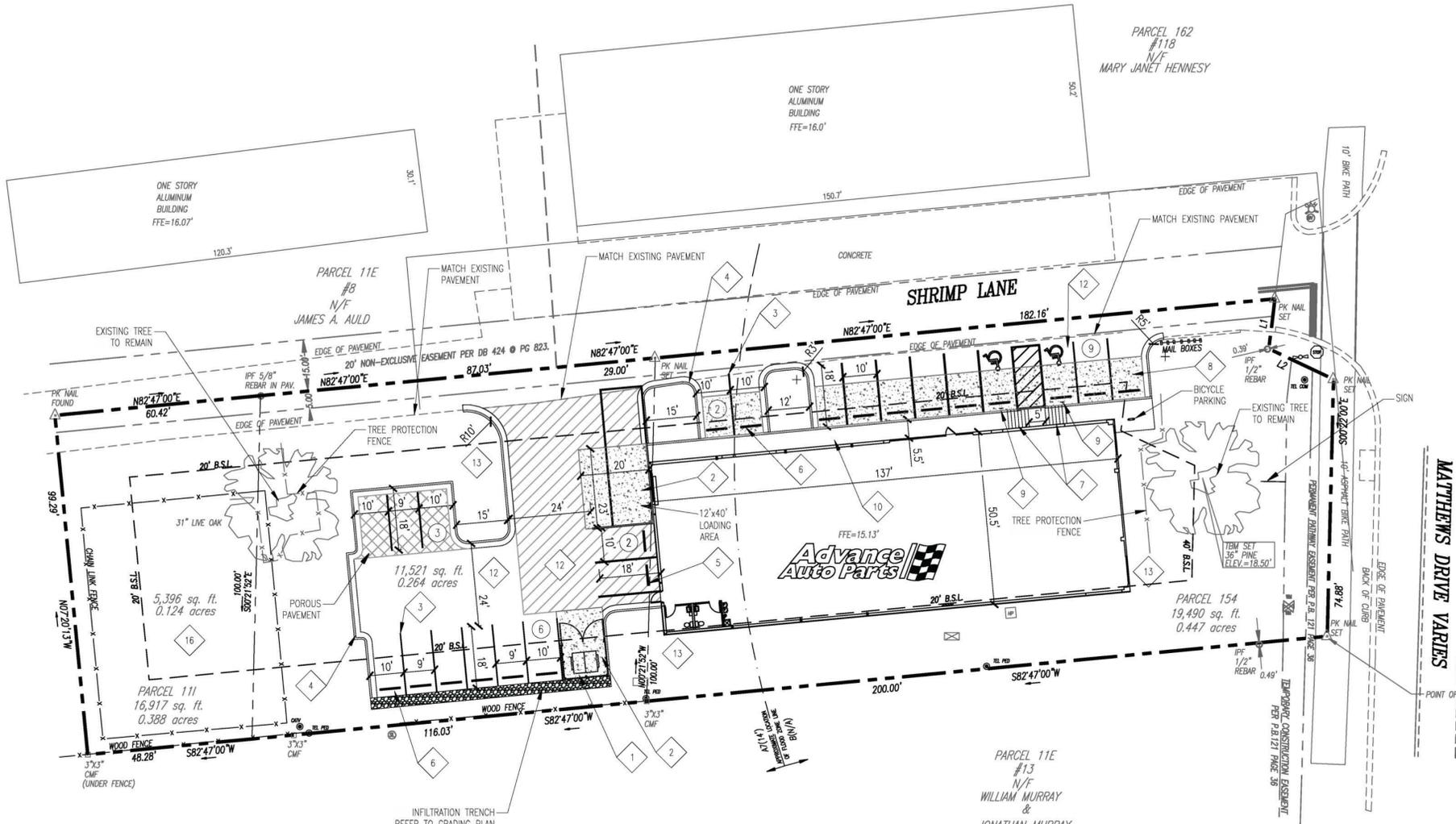
VERSION Q2-06

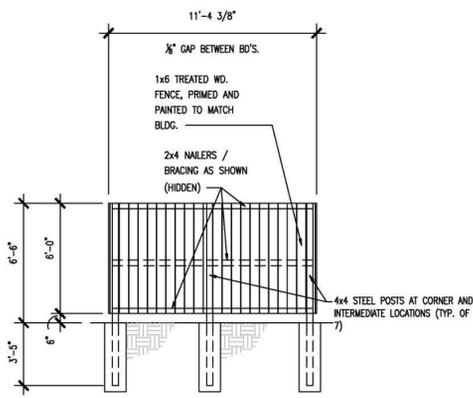
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Matthew Murray

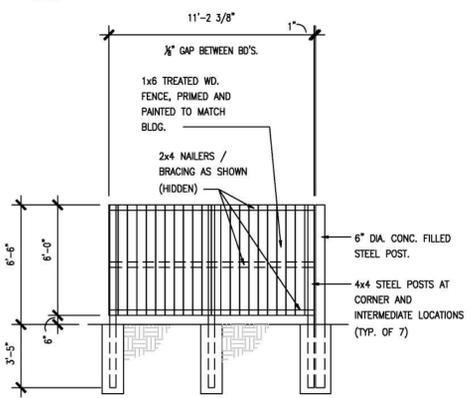
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AAP - HILTON HEAD, SC

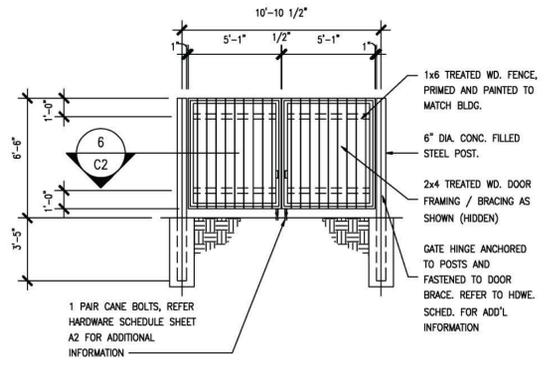




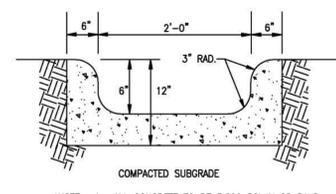
12 DUMPSTER FENCE ELEVATION (REAR)
C-2 NOT TO SCALE



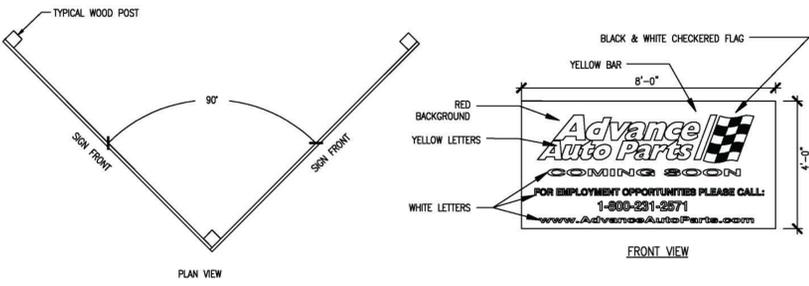
13 DUMPSTER FENCE ELEVATION (SIDE)
C-2 NOT TO SCALE



14 DUMPSTER GATE ELEVATION
C-2 NOT TO SCALE



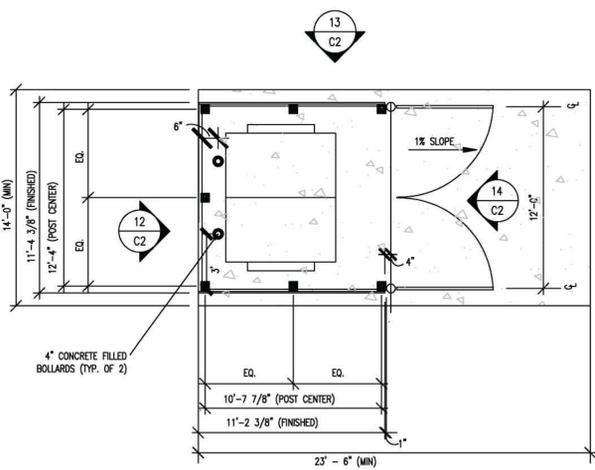
15 CONCRETE FLUME
C-2 NOT TO SCALE



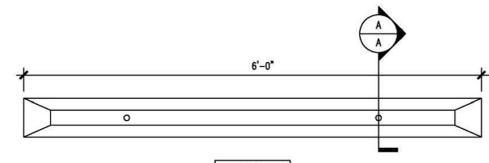
KEY TO SIGNAGE DETAILS

- CONTRACTOR IS TO PROVIDE TWO (2) 5/8" EXTERIOR GRADE PLYWOOD SIGNS FOR ADVANCE AUTO PARTS COMING SOON SIGNS ON 4" X 4" WOOD POSTS AT 90 DEGREE ANGLE. INSTALLATION OF SIGN IS TO OCCUR PRIOR TO START OF CONSTRUCTION OF BUILDING. IMPORTANT: CONTACT THE SIGN MANAGER AT ADVANCE AUTO PARTS FOR A PHOTOGRAPH OF ACCEPTABLE SIGN.
- IMPORTANT: SIGN MUST BE INSTALLED AS SOON AS POSSIBLE BEFORE CONSTRUCTION AND MUST BE REMOVED DURING THE WEEK OF MERCHANDISING.
- NOTE: THE USE OF PREFABRICATED 4" X 8" VINYL SIGN TO ACT AS AN ALTERNATIVE TO THE PLYWOOD SIGN WILL BE PERMITTED ONLY IF THE VINYL IS STRETCHED/PULLED TIGHT ON A PLYWOOD BACKING TO PREVENT ANY SLACK.
- SPECIFICATIONS:
RED BACKGROUND = RED TO MATCH PMS #485C/WHITE COPY = GREGORY #6550 PREMIUM WHITE OR WHITE VINYL/YELLOW COPY = YELLOW #2037 VINYL TO MATCH PMS #108C/CHECKERED LOGO = BLACK AND WHITE

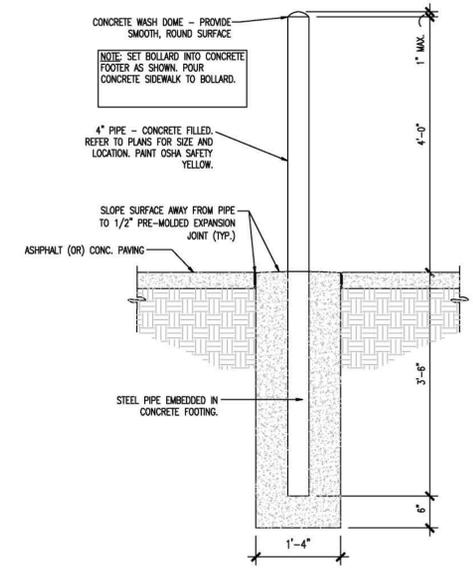
9 SIGNAGE DETAILS
C-2 NOT TO SCALE



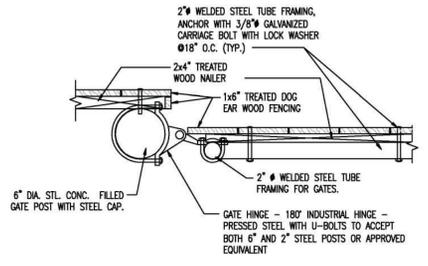
10 TYPICAL PAD LAYOUT
C-2 NOT TO SCALE



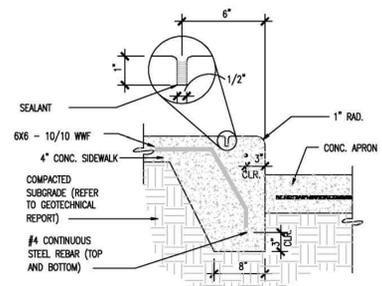
11 WHEEL STOP DETAIL
C-2 NOT TO SCALE



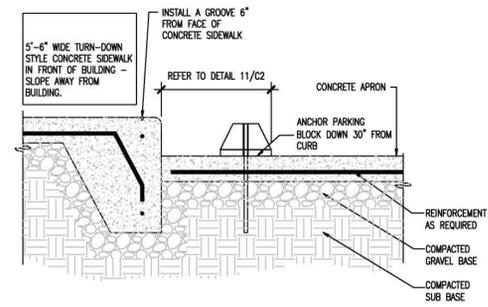
5 TYPICAL BOLLARD DETAIL
C-2 NOT TO SCALE



6 SECTION AT GATE POST
C-2 NOT TO SCALE

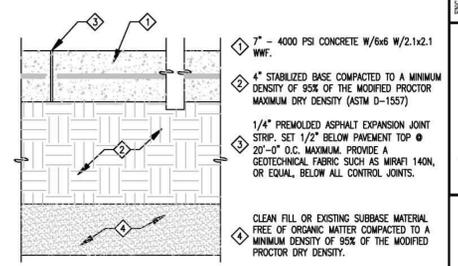


7 CURB EDGE CONCRETE SIDEWALK
C-2 NOT TO SCALE

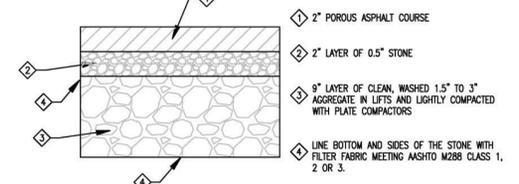


8 SIDEWALK DETAIL
C-2 NOT TO SCALE

- GENERAL CONCRETE NOTES:
RIGID PAVEMENT CONSISTS OF CONSTRUCTING A SPECIFIED PORTLAND CEMENT CONCRETE PAVING ON A PREPARED SUBGRADE. THE UTILITIES AND OTHER ITEMS IN AND BENEATH THE STREET MUST BE PROPERLY COORDINATED WITH THE CONSTRUCTION OF RIGID PAVEMENT TO AVOID CONFLICTS. THE WORK TO BE DONE SHALL INCLUDE THE FURNISHING OF ALL SUPERVISION, LABOR, MATERIALS, EQUIPMENT AND INCIDENTALS NECESSARY FOR THE PROPOSED RIGID PAVEMENT CONSTRUCTION IN ACCORDANCE WITH THE APPROVED DRAWINGS AND SPECIFICATIONS.
- CONCRETE STRENGTH REQUIRED:
ALL CONCRETE SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 4,000 (PO) PSI AT 28 DAYS. CONFORMANCE TO STRENGTH REQUIREMENTS SHALL BE DETERMINED BY ACI STANDARD 318, LATEST EDITION, SECTIONS 4.8.2.3 AND 4.8.3.



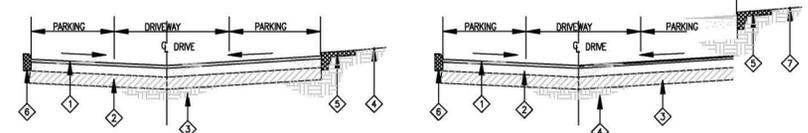
1 CONCRETE PAVEMENT SECTION
C-2 NOT TO SCALE



16 POROUS ASPHALT PAVEMENT
C-2 NOT TO SCALE

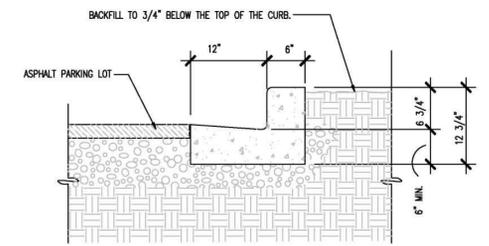
- KEY TO TYPICAL PAVEMENT SECTION (STANDARD DUTY)
- 2" ASPHALT SURFACE COURSE (SCOOT)
 - 7" GRADED AGGREGATE BASE (SCOOT AGGREGATE BASE COURSE)
 - CLEAN FILL OR EXISTING SUBBASE MATERIAL FREE OF ORGANIC MATTER
 - GRASS AREA
 - SIDEWALK
 - CURB AND GUTTER
 - WOVEN GEOTECHNICAL FABRIC (MINIMUM 10 OUNCES)

- KEY TO TYPICAL PAVEMENT SECTION (HEAVY DUTY)
- 1.5" ASPHALT SURFACE COURSE (SCOOT)
 - 2" ASPHALT INTERMEDIATE COURSE (SCOOT)
 - 8" AGGREGATE BASE COURSE (SCOOT AGGREGATE BASE COURSE)
 - CLEAN FILL OR EXISTING SUBBASE MATERIAL FREE OF ORGANIC MATTER
 - SIDEWALK
 - CURB AND GUTTER
 - GRASS AREA
 - WOVEN GEOTECHNICAL FABRIC (MINIMUM 10 OUNCE)



NOTE: ALL PAVEMENT MATERIALS SHALL BE IN ACCORDANCE WITH THE STATE DEPARTMENT OF TRANSPORTATION SPECIFICATIONS. PAVEMENT SECTION MUST MEET THE CRITERIA GIVEN IN THE SITE SOIL REPORT.

2 ASPHALT PAVEMENT SECTION
C-2 NOT TO SCALE



3 CURB/GUTTER DETAIL
C-2 NOT TO SCALE

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TOWN, NC 27611
HARTFORD COUNTY, NC

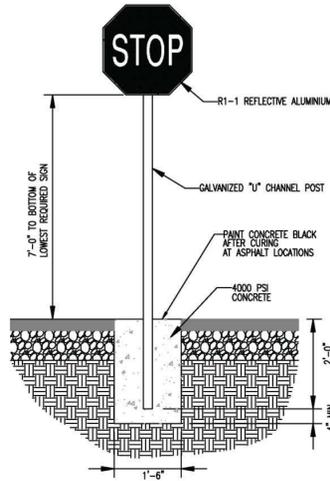
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7/30/12		6,845 S.F.
JOB #:		CHECK BY:
001016		
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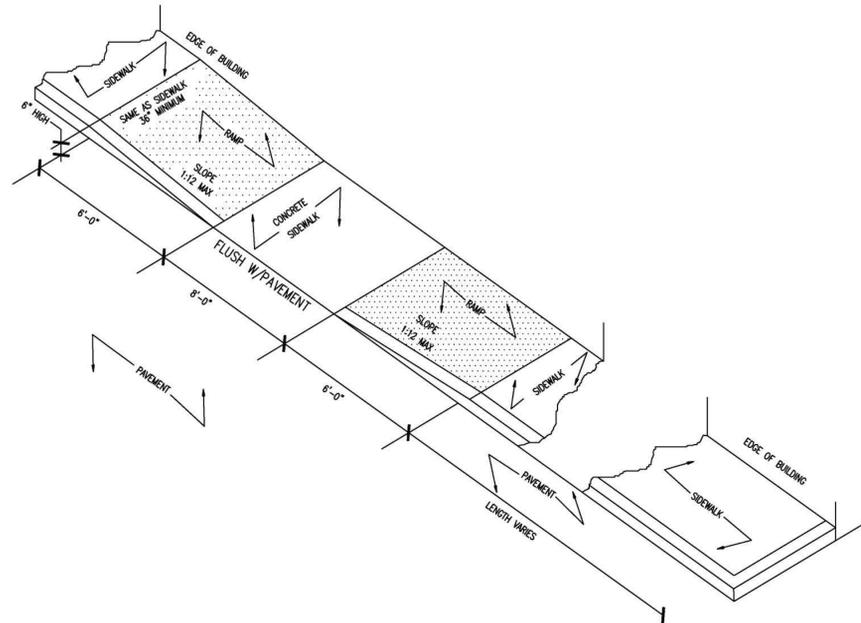
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South Carolina Professional Engineer
No. 21005
7/30/12
Matt Hind

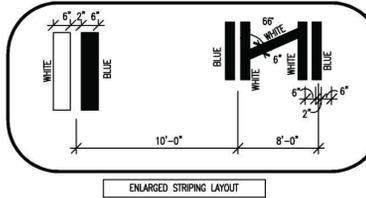
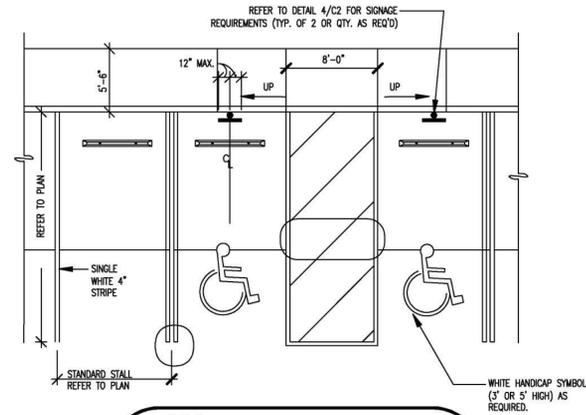
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8 STOP SIGN DETAIL
C-3 NOT TO SCALE

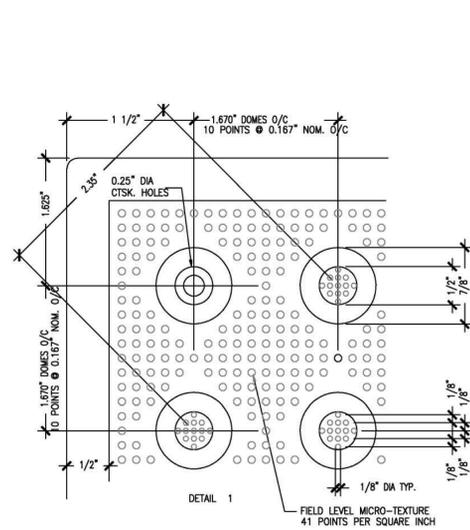


3 INLINE HANDICAP RAMP DETAIL
C-3 NOT TO SCALE



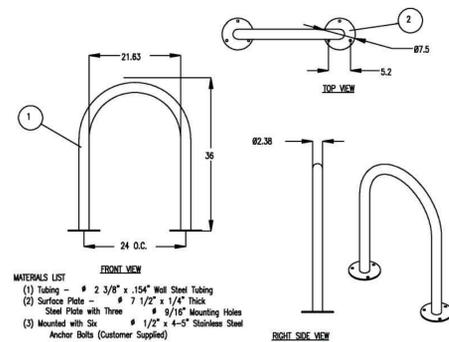
1 HANDICAP STRIPING PLAN
C-3 NOT TO SCALE

4 SIDEWALK END DETAIL
C-3 NOT TO SCALE



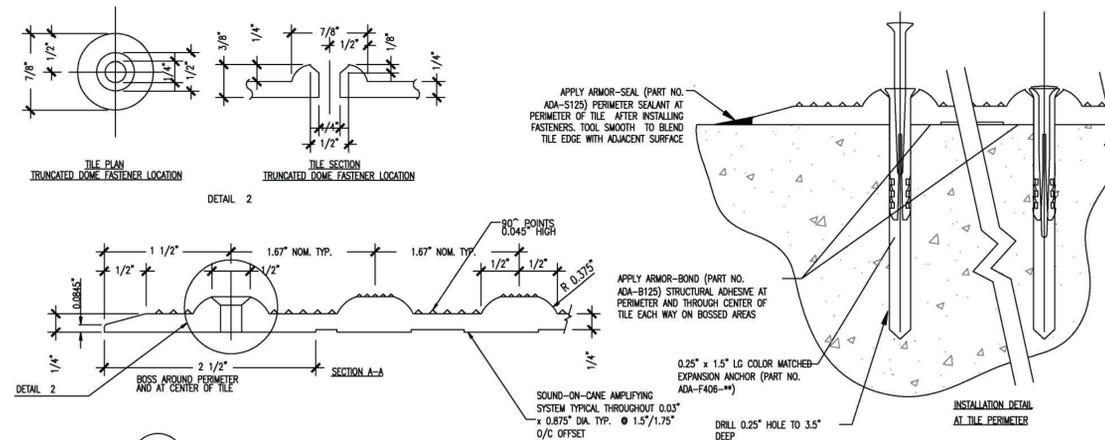
5 HANDICAP SIGNAGE
C-3 NOT TO SCALE

- NOTES:
1. ALL LETTERS ARE 1" SERIES "C" PER 2003 MUTCD.
 2. TOP PORTION OF SIGN SHALL HAVE A REFLECTORIZED (ENGINEERING GRADE) BLUE BACKGROUND WITH WHITE REFLECTORIZED LEGEND AND BORDER.
 3. BOTTOM PORTION OF SIGN SHALL HAVE A REFLECTORIZED (ENGINEERING GRADE) WHITE BACKGROUND WITH BLACK OPAQUE LEGEND AND BORDER.
 4. FINE NOTIFICATION SIGN SHALL HAVE A REFLECTORIZED (ENGINEERING GRADE) WHITE BACKGROUND WITH BLACK OPAQUE LEGEND AND BORDER.
- CONTRACTOR SHALL VERIFY FINE AMOUNT AND ORDINANCE NUMBER.
5. ONE (1) SIGN REQUIRED FOR EACH ACCESSIBLE PARKING SPACE.
 6. INSTALLED HEIGHT OF SIGN SHALL BE IN ACCORDANCE WITH SECTION 24-23 OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES. (MUTCD)
 7. SIGN MAY BE MOUNTED ON BUILDING/WALL, AT PROPER HEIGHT, IF ALIGNED WITHIN 12" OF CENTER OF PARKING SPACE.



- MATERIALS LIST
- (1) Tubing - # 2 3/8" x 1/4" Wall Steel Tubing
 - (2) Surface Plate - # 7 1/2" x 1/4" Thick
 - (3) Steel Plate with Three # 9/16" Mounting Holes
 - (4) Mounted with Six # 1/2" x 4-5" Stainless Steel
 - (5) Anchor Bolts (Customer Supplied)

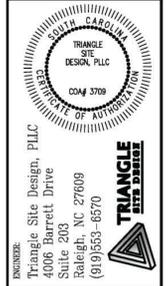
9 BICYCLE RACK DETAIL
C-3 NOT TO SCALE



6 WARNING SURFACE TILE
C-3 NOT TO SCALE

SITE ACCESSIBILITY DETAIL NOTES:

1. REFER TO SITE PLAN FOR EXACT LOCATION OF HANDICAP PARKING, WALKWAYS AND RAMP.
2. ALL WALKWAYS, RAMPS, AND HANDICAP PARKING SIGNAGE, ETC. SHALL MEET APPROVED AMERICANS WITH DISABILITIES ACT (ADA) STANDARDS AND LOCAL ACCESSIBILITY CODE.
3. ONE OUT OF EVERY EIGHT (8) ACCESSIBLE PARKING SPACES, BUT NOT LESS THAN ONE, IS REQUIRED TO BE VAN ACCESSIBLE.
4. ACCESSIBLE RAMPS
 - 4.1. ALL CONCRETE ACCESSIBLE RAMPS (3000 PSI) SURFACES SHALL HAVE A "WARNING SURFACE TILE" WITH SCREW DOWN TRUNCATED DOMES PER ADA STANDARDS AND LOCAL ACCESSIBILITY CODE(S).
 - 4.2. RAMPS ARE TO BE COLOR IMPREGNATED TO PROVIDE VISUAL CONTRAST (DARK TO LIGHT) WITH ADJACENT CONCRETE SURFACES (MIN. 70% COLOR CONTRAST).
5. FINISHED GRADES OF PAVEMENT IN HC PARKING AND ACCESSIBILITY AISLE SHALL NOT EXCEED 1:50 SLOPE IN ANY DIRECTION.



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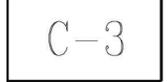
Advance Auto Parts
120 MATTHEWS DRIVE
HILTON HEAD
BEAUFORT COUNTY, SC
SITE ID #

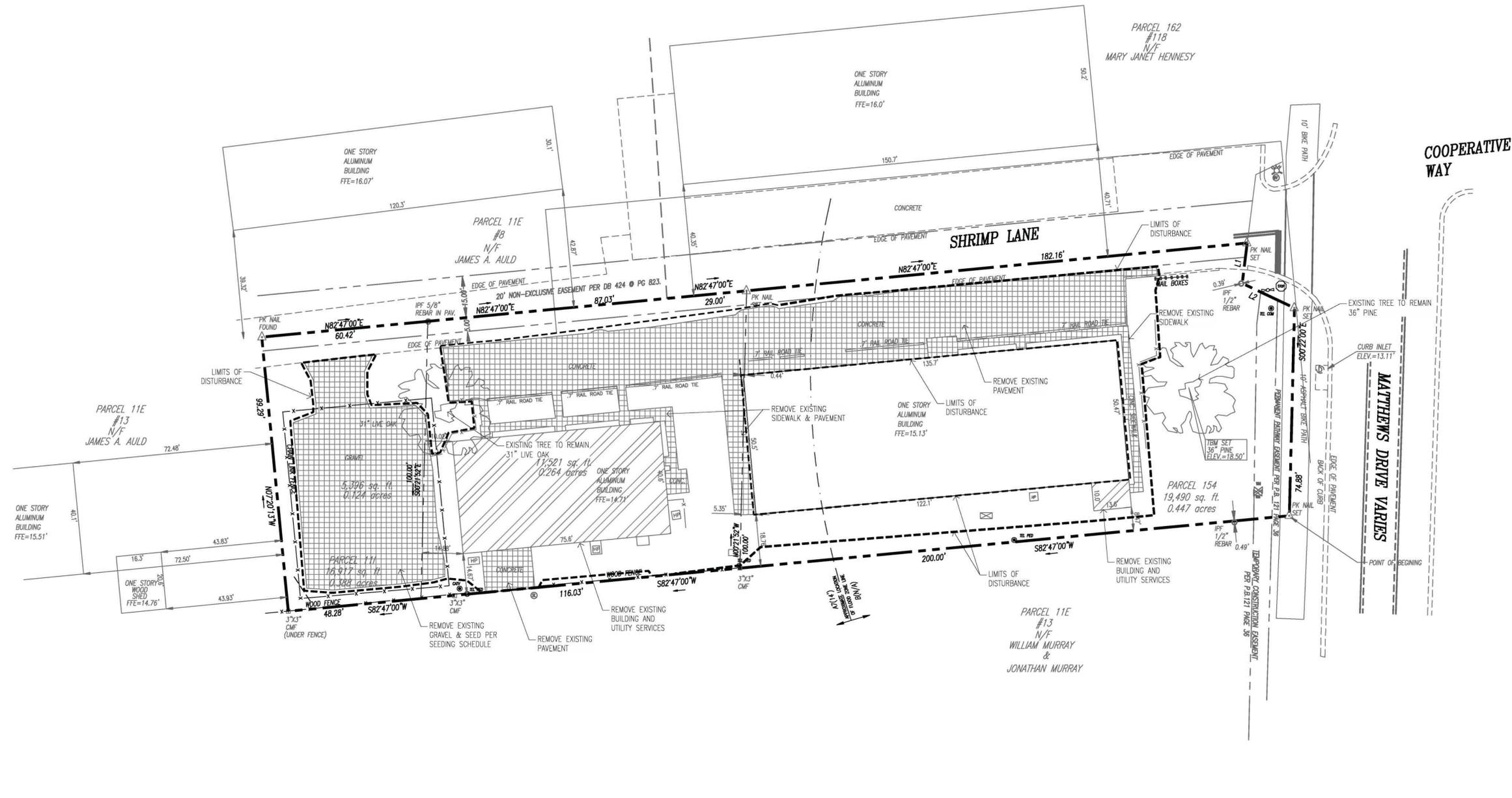
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SITE DETAILS

DATE: 7/30/12	SCALE: 8.845 S.F.
JOB #: 001016	DRAWN BY: CHECK BY:
VERSION 02-06	

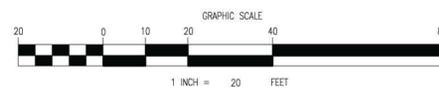
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DEMOLITION LEGEND

- BUILDINGS TO BE REMOVED
- PAVEMENT TO BE REMOVED
- UTILITIES TO BE REMOVED
- TREES TO BE REMOVED



CAUTIONARY NOTE

THE LOCATIONS OF ALL EXISTING UNDERGROUND UTILITIES SHOWN ON THIS DRAWING ARE BASED ON ABOVE GROUND EVIDENCE OR INFORMATION GATHERED DURING THE FIELD SURVEY PORTION OF THIS PROJECT. AS SUCH, THIS INFORMATION IS SPECULATIVE IN NATURE ONLY AND SHOULD NOT BE CONSTRUED AS FACT. THERE MAY ALSO BE OTHER EXISTING UNDERGROUND UTILITIES FOR WHICH NO ABOVE GROUND EVIDENCE WAS OBSERVED. THE EXACT LOCATIONS OF THE UNDERGROUND UTILITIES SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO THE START OF ANY AND ALL CONSTRUCTION.

UTILITY STATEMENT

THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION AND EXISTING DRAWINGS. THE SURVEYOR MAKES NO GUARANTEES THAT THE UNDERGROUND UTILITIES COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN-SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH THE SURVEYOR DOES CERTIFY THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM INFORMATION AVAILABLE. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES.

DEMOLITION NOTES:

1. THE CONTRACTOR SHALL FIELD VERIFY AND LOCATE ALL EXISTING UTILITIES ON SITE PRIOR TO DEMOLITION.
2. THE CONTRACTOR SHALL PERFORM DEMOLITION ACTIVITIES AS NOTED AND SHOWN ON THESE PLANS AND AS DIRECTED BY THE OWNER.
3. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO OBTAIN ANY PERMITS AND PAY FEES REQUIRED FOR DEMOLITION AND HAUL-OFF FROM THE APPROPRIATE AUTHORITIES.
4. THE CONTRACTOR SHALL PREPARE ALL DOCUMENTS AND ACQUIRE APPROPRIATE PERMITS AS REQUIRED PRIOR TO THE COMMENCEMENT OF DEMOLITION.
5. THE DEMOLITION PLAN IS INTENDED TO DEPICT GENERAL DEMOLITION AND UTILITY WORK. IT IS NOT INTENDED TO IDENTIFY EACH ELEMENT OF DEMOLITION OR RELOCATION. CONTRACTOR SHALL COORDINATE WITH THE OWNER AND APPROPRIATE UTILITY COMPANY PRIOR TO WORK.
6. CONTRACTOR TO COMPLETELY DEMOLISH AND DISPOSE OF OFFSITE IN A LAWFUL MANNER EXISTING BUILDINGS, INCLUDING FOUNDATIONS AND ALL APPURTENANCES LOCATED ON AND AROUND THE PROPERTY INCLUDING BUT NOT LIMITED TO BOLLARDS, GAS METERS, AIR CONDITIONING UNITS, SIGNS, CURBS, SIDEWALKS, ELECTRIC METERS, FENCING, ETC.
7. REMOVE AND DISPOSE OF ANY SIDEWALK, FENCES, STAIRS, WALLS, FOUNDATIONS, CONDUITS, LIGHT POLE BASES, DEBRIS AND RUBBISH REQUIRING REMOVAL FROM THE WORK AREA IN AN APPROVED LANDFILL.
8. REMOVE AND/OR PLUG EXISTING UTILITIES SUCH AS SANITARY SEWER, WATER, GAS, ELECTRIC, AND TELEPHONE AS SHOWN. THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING EACH UTILITY COMPANY TO COORDINATE REMOVAL OF ALL UTILITIES AND FOR DETERMINING HORIZONTAL AND VERTICAL LOCATIONS OF UTILITIES PRIOR TO COMMENCING WORK.
9. THE CONTRACTOR SHALL CUT AND PLUG, OR ARRANGE FOR THE APPROPRIATE UTILITY COMPANY TO CUT AND PLUG ALL SERVICE PIPING AT THE STREET LINE OR MAIN, AS REQUIRED, OR AS OTHERWISE NOTED. ALL SERVICES MAY NOT BE SHOWN ON THIS PLAN. THE CONTRACTOR SHALL INVESTIGATE THE SITE PRIOR TO BIDDING TO DETERMINE THE EXTEND OF SERVICE PIPING TO BE REMOVED, CUT OR PLUGGED.
10. THE CONTRACTOR SHALL ARRANGE FOR RESETTING OF CURB BOXES, VALVE BOXES AND REMOVAL AND/OR RELOCATION OF OVERHEAD UTILITIES AND POLES WITH THE APPROPRIATE UTILITY COMPANY.
11. INSTALL ALL EROSION AND SEDIMENT CONTROL DEVICES AND TREE PROTECTION PRIOR TO BEGINNING DEMOLITION WORK.
12. THE CONTRACTOR IS RESPONSIBLE FOR THE PROTECTION OF ALL UTILITIES TO REMAIN IN PLACE.
13. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO AVOID UNNECESSARY DAMAGE TO EXISTING ROAD SURFACE. FINISH SURFACE TO BE REMOVED OR DEMOLISHED SHALL BE CUT ALONG LINES OF JOINTS WHICH WILL PERMIT A NEAT SURFACE WHEN RESTORED.
14. SAWCUT AT INTERFACE OF PAVEMENT OR CURB TO REMAIN. SAWCUT EXISTING PAVEMENT.
15. ALL EXISTING ITEMS TO REMAIN WHICH ARE DAMAGED DURING CONSTRUCTION SHALL BE RESTORED TO ITS ORIGINAL CONDITION AT THE SOLE EXPENSE OF THE CONTRACTOR.
16. DO NOT INTERRUPT EXISTING UTILITIES SERVING FACILITIES OCCUPIED AND USED BY THE OWNER OR OTHERS DURING OCCUPIED HOURS EXCEPT WHEN SUCH INTERRUPTIONS HAVE BEEN AUTHORIZED IN WRITING BY THE OWNER AND THE LOCAL MUNICIPALITIES. INTERRUPTIONS SHALL ONLY OCCUR AFTER ACCEPTABLE TEMPORARY SERVICE HAS BEEN PROVIDED.
17. SHOULD ANY UNCHARTERED OR INCORRECTLY CHARTERED EXISTING PIPING OR OTHER UTILITY BE UNCOVERED DURING EXCAVATION, CONSULT THE ENGINEER IMMEDIATELY FOR DIRECTIONS BEFORE PROCEEDING FURTHER WITH WORK IN THIS AREA.
18. ASBESTOS OR HAZARDOUS MATERIAL, IF FOUND ON SITE, SHALL BE REMOVED BY A LICENSED HAZARDOUS MATERIAL CONTRACTOR.
19. THE PROPERTY SELLER SHALL PUMP OUT BUILDING FUEL, GREASE TRAPS, AND WASTE OIL TANKS (IF ANY ARE ENCOUNTERED) AND REMOVE FUEL TO AN APPROVED DISPOSAL AREA BY AN APPROPRIATELY LICENSED WASTE OIL HANDLING CONTRACTOR IN STRICT ACCORDANCE WITH FEDERAL AND STATE REQUIREMENTS BEFORE CONSTRUCTION BEGINS.
20. THE CONTRACTOR SHALL PROPERLY AND LEGALLY DISPOSE OF ALL DEMOLITION DEBRIS OFF OF THE SITE.



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OWNER/DEVELOPER:
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 ELGIN, SC 29045
 803-361-8718
 collins@hartzogholdings.com

TOWN OF HILTON HEAD
 BEAUFORT COUNTY, SC

Advance Auto Parts
 SITE ID #

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DEMOLITION PLAN

DATE: 7/30/12
 SHEET NO.: 2
 TOTAL SHEETS: 6,845 S.F.

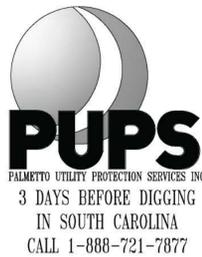
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Matthew A. Hind

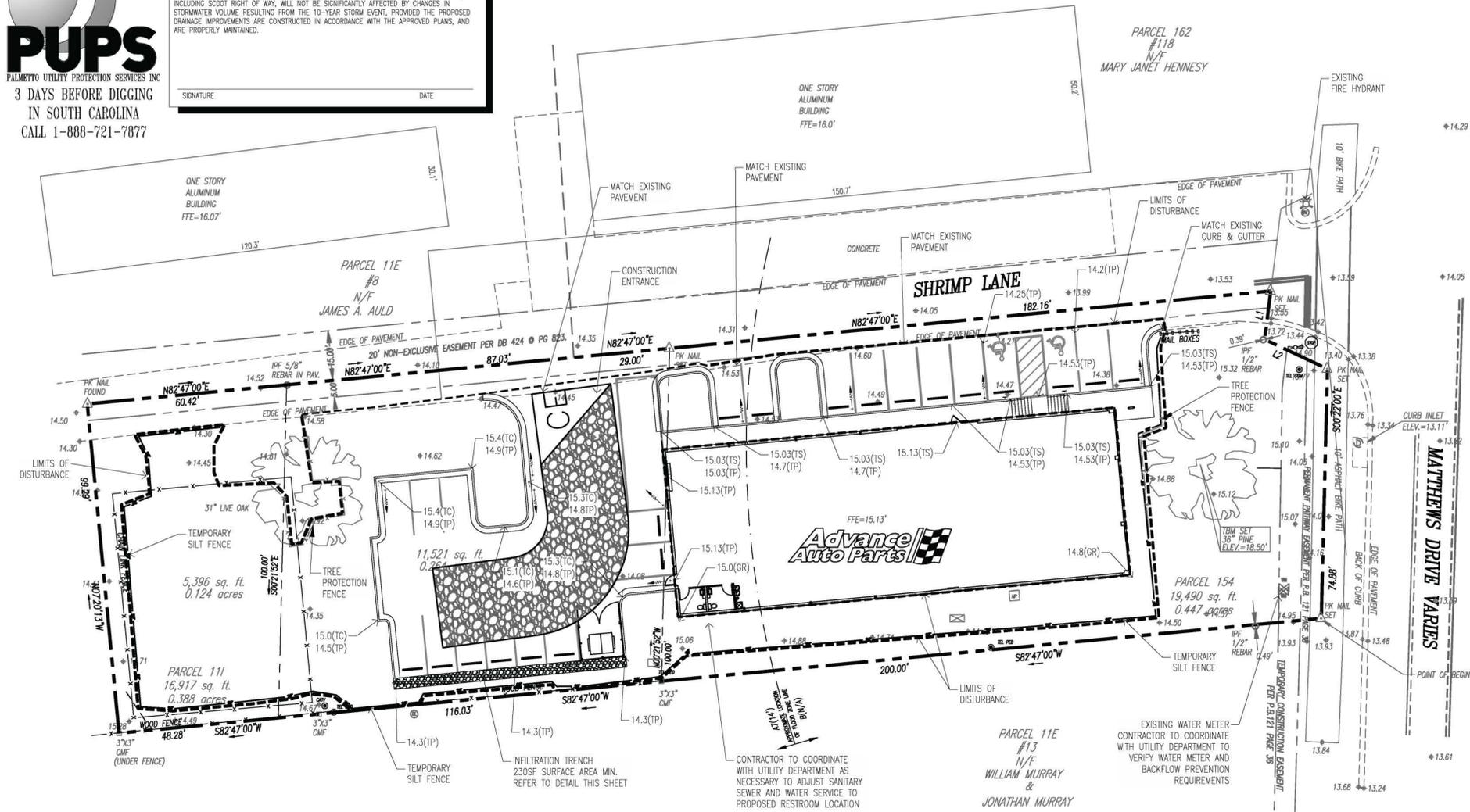
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DRAINAGE STATEMENT

BASED UPON THE INFORMATION AVAILABLE TO ME AND THE RESULTS OF A SOUND ENGINEERING ANALYSIS, IT IS MY PROFESSIONAL OPINION THAT THIS PLAN COMPLIES WITH APPLICABLE STATE AND LOCAL STORM WATER MANAGEMENT REQUIREMENTS, AND THAT PROPERTIES DOWNSTREAM, INCLUDING SCOOT RIGHT OF WAY, WILL NOT BE SIGNIFICANTLY AFFECTED BY CHANGES IN STORMWATER VOLUME RESULTING FROM THE 10-YEAR STORM EVENT, PROVIDED THE PROPOSED DRAINAGE IMPROVEMENTS ARE CONSTRUCTED IN ACCORDANCE WITH THE APPROVED PLANS, AND ARE PROPERLY MAINTAINED.

SIGNATURE _____ DATE _____



GRADING NOTES:

- REFER TO THE SITE PLAN FOR RELATED NOTES.
- ALL CONTOURS AND SPOT ELEVATIONS REFLECT FINISHED GRADES.
- ALL ELEVATIONS ARE IN REFERENCE TO THE BENCHMARK, AND THIS MUST BE VERIFIED BY THE GENERAL CONTRACTOR PRIOR TO GROUND BREAKING.
- THE CONTRACTOR SHALL IMMEDIATELY REPORT TO OWNER ANY DISCREPANCIES FOUND BETWEEN ACTUAL FIELD CONDITIONS AND CONSTRUCTION DOCUMENTS AND SHALL WAIT FOR INSTRUCTION PRIOR TO PROCEEDING.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND PROTECTING EXISTING UTILITIES, AND SHALL REPAIR ALL DAMAGE TO EXISTING UTILITIES THAT OCCUR DURING CONSTRUCTION.
- THE CONTRACTOR SHALL BLEND NEW EARTHWORK SMOOTHLY TO TRANSITION BACK TO EXISTING GRADE.
- LIMITS OF CLEARING SHOWN ON GRADING PLAN ARE BASED UPON THE APPROXIMATE CUT AND FILL SLOPE LIMITS, OR OTHER GRADING REQUIREMENTS.
- THE PROPOSED CONTOURS SHOWN IN DRIVES AND PARKING LOTS AND SIDEWALKS ARE FINISHED ELEVATIONS INCLUDING ASPHALT. REFER TO PAVEMENT CROSS SECTION DATA TO ESTABLISH CORRECT SUBGRADE OR AGGREGATE BASE COURSE ELEVATIONS TO BE COMPLETED UNDER THIS CONTRACT.
- THE CONTRACTOR SHALL ENSURE POSITIVE DRAINAGE SO THAT RUNOFF WILL DRAIN BY GRAVITY FLOW ACROSS NEW PAVEMENT AREAS TO NEW OR EXISTING DRAINAGE INLETS OR SHEET OVERLAND.
- ANY GRADING BEYOND THE LIMITS OF CONSTRUCTION AS SHOWN ON THE GRADING PLAN IS SUBJECT TO A FINE.
- GRADING MORE THAN ONE ACRE WITHOUT AN APPROVED EROSION CONTROL PLAN IS SUBJECT TO A FINE.
- STABILIZATION IS THE BEST FORM OF EROSION CONTROL. TEMPORARY SEEDING IS NECESSARY TO ACHIEVE EROSION CONTROL ON DENuded AREAS AND ESPECIALLY WHEN THE CONSTRUCTION SEQUENCE REQUIRES IT.
- ALL GRADED SLOPES ARE TO BE SEED OR LANDSCAPED WITHIN 15 DAYS OF COMPLETION OF GRADING. ALL REMAINING AREAS ARE TO BE SEED WITHIN 30 DAYS.
- EXISTING GRASSES, CONTOURS, UTILITIES AND OTHER EXISTING FEATURES FROM SURVEY BY 1-SQUARE GROUP, INC.
- THE CONTRACTOR SHALL INCLUDE IN THE CONTRACT PRICE ANY DE-WATERING NECESSARY TO CONSTRUCT THE PROJECT AS SHOWN ON THE PLANS.
- THE CONTRACTOR IS RESPONSIBLE FOR THE DESIGN AND IMPLEMENTATION OF ALL SHEETING, SHORING, BRACING AND SPECIAL EXCAVATION MEASURES REQUIRED TO MEET OSHA, FEDERAL, STATE, AND LOCAL REGULATIONS PERSUANT TO THE INSTALLATION OF THE WORK INDICATED ON THESE DRAWINGS. THE DESIGN ENGINEER ACCEPTS NO RESPONSIBILITY FOR THE DESIGN(S) TO INSTALL SAID ITEMS.
- THE CONTRACTOR SHALL REFER TO THE ARCHITECTURAL PLANS FOR EXACT LOCATION, ELEVATION, AND DIMENSIONS OF EXIST. DOORS, RAMPS, BUILDING DIMENSIONS, AND EXACT BUILDING UTILITY ENTRANCE LOCATIONS.
- ALL FILL MATERIALS, EXISTING BUILDING FOUNDATIONS, PAVEMENT AND UTILITY STRUCTURES, TOPSOIL, AND ANY OTHER DELICATE MATERIALS SHALL BE COMPLETELY REMOVED FROM WITHIN THE BEARING ZONE BELOW THE STRUCTURE.
- ALL FOUNDATION EXCAVATION SHALL BE INSPECTED BY A QUALIFIED GEOTECHNICAL REPRESENTATIVE TO DETERMINE WHETHER UNSUITABLE MATERIAL MUST BE REMOVED. ALL UNSUITABLE MATERIAL SHALL BE REMOVED, BACKFILLED AND COMPACTED AS REQUIRED BY THE GEOTECHNICAL REPRESENTATIVE.
- ALL CUT OR FILL SLOPES SHALL BE 3:1 OR FLATTER UNLESS OTHERWISE NOTED OR DEPICTED. CONSTRUCTION SHALL ADHERE TO ALL TERMS & CONDITIONS AS OUTLINED IN THE GENERAL N.P.D.S PERMIT FOR STORMWATER DISCHARGE ASSOCIATED WITH CONSTRUCTION ACTIVITIES.
- THE CONTRACTOR SHALL ADJUST AND/OR CUT EXISTING PAVEMENT AS NECESSARY TO ASSURE A SMOOTH FIT AND CONTINUOUS GRADE.
- CONTRACTOR SHALL ENSURE POSITIVE DRAINAGE AWAY FROM BUILDINGS FOR ALL NATURAL AND PAVED AREAS.
- ALL UNSURFACED AREAS DISTURBED BY GRADING OPERATION SHALL RECEIVE 4 INCHES OF TOPSOIL. CONTRACTOR SHALL APPLY STABILIZATION FABRIC TO ALL SLOPES 3:1:1 OR STEEPER.
- CONSTRUCTION SHALL COMPLY WITH ALL APPLICABLE COVERING CODES AND BE CONSTRUCTED TO SAME.

UTILITY NOTES:

- THE CONTRACTOR IS FULLY RESPONSIBLE FOR CONTACTING APPROPRIATE PARTIES AND ASSURING THAT EXISTING UTILITIES ARE LOCATED PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. CONTACT THE LOCAL MUNICIPALITY PUBLIC WORKS DEPARTMENT 48 HOURS PRIOR TO COMMENCEMENT OF WORK FOR UTILITY LOCATING SERVICES.
- CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH THE LOCAL MUNICIPALITY PUBLIC UTILITIES STANDARDS.
- CONTRACTOR IS RESPONSIBLE FOR COMPLYING TO THE SPECIFICATIONS OF THE LOCAL AUTHORITIES WITH REGARDS TO MATERIALS AND INSTALLATION OF THE WATER AND SEWER LINES.
- CONTRACTOR SHALL COORDINATE WITH ALL UTILITY COMPANIES FOR INSTALLATION REQUIREMENTS AND SPECIFICATIONS.
- CONTRACTOR SHALL NOTIFY THE UTILITY AUTHORITIES INSPECTOR 72 HOURS BEFORE CONNECTING TO ANY EXISTING LINE.
- THE CONTRACTOR SHALL COORDINATE ANY INTERRUPTION OF UTILITY SERVICE WITH THE OWNER AND THE UTILITY COMPANY. ANY PLANNED INTERRUPTION OF UTILITY SERVICE SHALL BE GIVEN A 48 HOUR NOTICE TO THE UTILITY COMPANY AND THE OWNER.
- SHOULD ANY UNCHARTERED OR INCORRECTLY CHARTERED UTILITIES BE ENCOUNTERED, THE CONTRACTOR SHALL CONTACT THE OWNER IMMEDIATELY FOR DIRECTIONS.
- PRESSURE REDUCING VALVES WILL BE REQUIRED ON THE DOMESTIC WATERLINES FOR EACH BUILDING IF THE STATIC PRESSURE IN THE WATERMAIN EXCEEDS 80 PSI. SEE MECHANICAL/PLUMBING PLANS.
- ROUTES SHOWN FOR WATER SERVICES, ELECTRIC, SANITARY SEWER BUILDING LATERALS AND ROOF DRAIN PIPING ARE SUBJECT TO CHANGE.
- THE CONTRACTOR SHALL SAW CUT, REMOVE, AND REPLACE ASPHALT PAVEMENT AS NECESSARY TO INSTALL UNDERGROUND ELECTRIC, TELEPHONE, SEWER, AND WATER.

SCHEIC STANDARD NOTES:

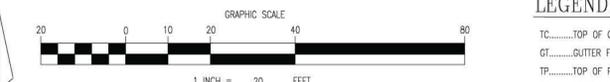
- IF NECESSARY, SLOPES WHICH EXCEED (8) VERTICAL FEET SHOULD BE STABILIZED WITH SYNTHETIC OR VEGETATIVE MATS IN ADDITION TO TERRACING. IT MAY BE NECESSARY TO INSTALL TEMPORARY SLOPE DRAINS DURING CONSTRUCTION. TEMPORARY BEAMS MAY BE NEEDED UNTIL THE SLOPE IS STABILIZED BY SEEDING OR VEGETATION.
- STABILIZATION MEASURES SHALL BE INSTALLED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN FOURTEEN (14) DAYS AFTER WORK HAS CEASED, EXCEPT AS SHOWN OTHERWISE BELOW.
 - WHERE STABILIZATION IS REQUIRED BY THE 14th DAY OF STOPPING OF WORK UNDER CONSTRUCTION CONDITIONS STABILIZATION MEASURES MUST BE INSTALLED AS SOON AS PRACTICABLE.
 - WHERE CONSTRUCTION OCCURS ON A PORTION OF THE SITE IS TEMPORARILY CEASED, AND EARTH-MOVING ACTIVITIES WILL BE RESUMED WITHIN 14 DAYS, TEMPORARY STABILIZATION MEASURES DO NOT NEED TO BE INSTALLED ON THAT PORTION OF THE SITE.
- ALL SEDIMENT AND EROSION CONTROL DEVICES SHALL BE INSPECTED EVERY SEVEN (7) DAYS IF SITE INSPECTION IDENTIFY IMPACTS THAT ARE DAMAGED OR ARE NOT OPERATING EFFECTIVELY. MAINTENANCE MUST BE PERFORMED AS SOON AS PRACTICAL OR AS REASONABLY POSSIBLE AND BEFORE THE NEXT STORM EVENT WHENEVER PRACTICABLE.
- ALL SEDIMENT AND EROSION CONTROL DEVICES SHALL BE INSPECTED AT LEAST EVERY FOURTEEN (14) CALENDAR DAYS AND WITHIN 24 HOURS OF THE END OF A STORM EVENT OF 0.5 INCHES OR GREATER. IF SITE INSPECTION IDENTIFY IMPACTS THAT ARE DAMAGED OR ARE NOT OPERATING EFFECTIVELY, MAINTENANCE MUST BE PERFORMED AS SOON AS PRACTICAL OR AS REASONABLY POSSIBLE AND BEFORE THE NEXT STORM EVENT WHENEVER PRACTICABLE.
- PROVIDE SILT FENCE AND/OR OTHER CONTROL DEVICES AS MAY BE REQUIRED TO CONTROL SOIL EROSION DURING UTILITY CONSTRUCTION. ALL DISTURBED AREAS SHALL BE CLEANED AND STABILIZED WITH GRASSING IMMEDIATELY AFTER UTILITY INSTALLATION. FILL, CONTROL, AND TEMPORARY SEEDING AT THE END OF EACH DAY ARE RECOMMENDED. IF WATER IS ENCOUNTERED WHILE TRENCHING, THE WATER SHOULD BE ALLOWED TO SEEP INTO THE TRENCH BEFORE BEING PUMPED BACK INTO ANY WADDS OF THE SITE.
- ALL EROSION CONTROL DEVICES SHALL BE PROPERLY MAINTAINED DURING ALL PHASES OF CONSTRUCTION UNTIL THE COMPLETION OF ALL CONSTRUCTION ACTIVITIES AND ALL DISTURBED AREAS HAVE BEEN STABILIZED. ADDITIONAL CONTROL DEVICES MAY BE REQUIRED DURING CONSTRUCTION IN ORDER TO CONTROL EROSION AND/OR OFF-SITE SEDIMENTATION. ALL TEMPORARY CONTROL DEVICES SHALL BE REMOVED ONCE CONSTRUCTION IS COMPLETE AND THE SITE IS STABILIZED.
- THE CONTRACTOR MUST TAKE NECESSARY ACTION TO MINIMIZE THE TRACKING OF MUD ONTO PAVED ROADWAYS FROM CONSTRUCTION AREAS AND THE GENERATION OF SOOT. THE CONTRACTOR SHALL DAILY WASH MUD OFF FROM PAVEMENT, AS MAY BE REQUIRED.
- RESIDENTIAL SUBDIVISIONS REQUIRING EROSION CONTROL FEATURES FOR INFRASTRUCTURE AS WELL AS FOR INDIVIDUAL LOT CONSTRUCTION, INDIVIDUAL PROPERTY OWNERS SHALL FOLLOW THESE PLANS DURING CONSTRUCTION OR OBTAIN APPROVAL OF AN INDIVIDUAL PLAN IN ACCORDANCE WITH S.C. REG. 72-300 ET SEQ. AND SUBORDINATE.
- TEMPORARY DIVERSION BEINGS AND/OR DITCHES WILL BE PROVIDED AS NEEDED DURING CONSTRUCTION TO PROTECT WORK AREAS FROM UPSLOPE RUNOFF AND/OR TO DIVERT SEDIMENT-LADEN WATER TO APPROPRIATE TRAPS OR STABLE OUTLETS.
- ALL INTERIERS OF THE SITE (NOTES, INCLUDING WETLANDS, ARE TO BE FLAGGED OR OTHERWISE CLEARLY MARKED IN THE FIELD. A DOUBLE ROW OF SILT FENCE IS TO BE INSTALLED IN ALL AREAS WHERE A 50-FOOT BUFFER MUST BE MAINTAINED BETWEEN THE DISTURBED AREA AND ALL WAYS. A 10-FOOT BUFFER SHOULD BE MAINTAINED BETWEEN THE LAST ROW OF SILT FENCE AND ALL WAYS.
- LITTER, CONSTRUCTION DEBRIS, OILS, FUELS, AND BUILDING PRODUCTS WITH SIGNIFICANT POTENTIAL FOR IMPACT (SUCH AS STOCKPILES OF FRESHLY TREATED LUMBER) AND CONSTRUCTION CHEMICALS THAT COULD BE EXPOSED TO STORM WATER MUST BE PREVENTED FROM BECOMING A POLLUTANT SOURCE IN STORM WATER DISCHARGE.

EROSION CONTROL NOTES:

- TOTAL AREA DISTURBED = 0.42 ACRES. TOTAL SITE AREA = 6.04 ACRES.
- UNLESS OTHERWISE INDICATED, ALL VEGETATIVE AND STRUCTURAL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE CONSTRUCTED AND MAINTAINED IN ACCORDANCE WITH THE MINIMUM STANDARDS AND SPECIFICATIONS OF THE SCHEIC EROSION AND SEDIMENT CONTROL HANDBOOK.
- THE CONTRACTOR SHALL INSTALL AND MAINTAIN THROUGHOUT THE PROJECT CONSTRUCTION ALL EROSION CONTROL MEASURES SHOWN WITH THESE PLANS IN ACCORDANCE WITH APPLICABLE SOUTH CAROLINA EROSION AND SEDIMENT CONTROL REGULATIONS.
- CONSTRUCTION WORK SHALL BE IN COMPLIANCE WITH REGULATIONS OF THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) STORM WATER GENERAL PERMIT.
- EROSION AND SEDIMENT CONTROL MEASURES ARE TO BE PLACED PRIOR TO CLEARING AND/OR LAND DISTURBANCE.
- A COPY OF THE APPROVED EROSION AND SEDIMENT CONTROL PLAN AND PERMIT SHALL BE MAINTAINED ON THE SITE AT ALL TIMES.
- THE CONTRACTOR SHALL DILIGENTLY AND CONTINUOUSLY MAINTAIN ALL EROSION CONTROL DEVICES AND STRUCTURES TO MINIMIZE EROSION. THE CONTRACTOR SHALL MAINTAIN CLOSE CONTACT WITH THE SCHEIC AND/OR TOWN EROSION CONTROL INSPECTOR SO THAT PERIODIC INSPECTIONS CAN BE PERFORMED AT APPROPRIATE SPACES OF CONSTRUCTION.
- APPROVAL OF THIS PLAN IS NOT AN AUTHORIZATION TO GRADE ADJACENT PROPERTIES. WHEN FIELD CONDITIONS WARRANT OFF-SITE GRADING, PERMISSION MUST BE OBTAINED FROM THE AFFECTED PROPERTY OWNERS. A REVISED PLAN SHOWING OFF-SITE IMPACTS SHOULD BE SUBMITTED AND APPROVED PRIOR TO ANY OFF-SITE GRADING. CONTACT PROJECT ENGINEER AND PROJECT EROSION CONTROL INSPECTOR TO ENSURE ADDITIONAL EROSION CONTROL MEASURES ARE INSTALLED PRIOR TO OFF-SITE GRADING.
- CONTRACTOR TO COMMENCE LAND DISTURBING ACTIVITIES IN AREAS OTHER THAN INDICATED ON THESE PLANS (INCLUDING, BUT NOT LIMITED TO OFF-SITE EROSION OR WASTE AREAS, STAGING OR STORAGE AREAS). THE CONTRACTOR SHALL PREPARE AND SUBMIT A SUPPLEMENTARY EROSION CONTROL PLAN TO THE OWNER FOR REVIEW AND TO SCHEIC AND/OR TOWN FOR APPROVAL. CONTRACTOR SHALL PAY ALL FEES REQUIRED AND SHALL OBTAIN NECESSARY PERMITS AND NO DAMAGE PERMIT. THE CONTRACTOR SHALL PROVIDE THE OWNER AND THE ENGINEER A COPY OF THE APPROVED PERMIT.
- EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED CONTINUOUSLY, RELOCATED WHEN AND AS NECESSARY, AND SHALL BE CHECKED AFTER EVERY RAINFALL. SEEDING AREAS SHALL BE CHECKED REGULARLY AND SHALL BE RESEED, FERTILIZED, AND SEEDING AND MULCHED AS NECESSARY TO OBTAIN A DENSE STAND OF GRASS.
- STABILIZATION IS THE BEST FORM OF EROSION CONTROL. ALL DISTURBED AREAS WHICH ARE NOT OTHERWISE STABILIZED SHALL BE TOP SOILED AND SEEDED. TEMPORARILY OR PERMANENTLY BY ACCORDANCE WITH THE SCHEIC AND TOWN SEDIMENT CONTROL REGULATIONS. PERMANENT SEEDING AND GRASS ESTABLISHMENT IS REQUIRED PRIOR TO PROJECT COMPLETION AND ACCEPTANCE.
- WHEN A DESIGNATED STONE CONSTRUCTION ENTRANCE HAS BEEN COVERED WITH SOIL OR HAS BEEN PUSHED INTO THE SOIL BY CONSTRUCTION TRAFFIC, IT SHALL BE REPLACED WITH A PILE OF STONE EQUAL TO THAT OF THE ORIGINAL APPLICATOR.
- TEMPORARY GRAVEL CONSTRUCTION ENTRANCE SHALL BE REQUIRED AT ALL CONSTRUCTION STAGING AREA ENTRANCES AND ALL CONSTRUCTION ACCESS LOCATIONS AND NON-PAVED AREAS 20 INCHES OF STONE SHALL BE USED FOR THE TEMPORARY GRAVEL CONSTRUCTION ENTRANCE.
- ALL DRAINAGE ALLEYS SHALL BE PROTECTED FROM SILTATION. EFFECTIVE PROTECTION DEVICES SHALL BE MAINTAINED AND SHALL BE CLEANED. FLAGGING IS NOT AN ACCEPTABLE METHOD OF CLEANING.
- SEDIMENT BASINS AND TRAPS, PERMITS, DATES, SEDIMENT BARRIERS AND OTHER MEASURES INTENDED TO TRAP SEDIMENT SHALL BE CONSTRUCTED AS A FIRST STEP IN ANY LAND-DISTURBING ACTIVITY AND SHALL BE MADE FUNCTIONAL BEFORE UP-SLOPE SOIL DISTURBANCE TAKES PLACE.
- ALL DISTURBED AREAS ARE TO DRAIN TO APPROVED SEDIMENT CONTROL MEASURES AT ALL TIMES DURING LAND DISTURBING ACTIVITIES AND DURING SITE DEVELOPMENT UNTIL FINAL STABILIZATION IS COMPLETED.
- DURING DE-WATERING OPERATIONS, WATER SHALL BE PUMPED INTO AN APPROVED FILTERING DEVICE PRIOR TO DISCHARGE TO RECEIVING OUTLET.
- THE CONTRACTOR SHALL INSPECT ALL EROSION CONTROL MEASURES PERIODICALLY AND AFTER EACH RAINFALL-PRODUCING EVENT. ANY NECESSARY REPAIRS OR CLEANUP TO MAINTAIN THE EFFECTIVENESS OF THE EROSION CONTROL DEVICES SHALL BE MADE IMMEDIATELY.

BACKFLOW PREVENTION NOTES:

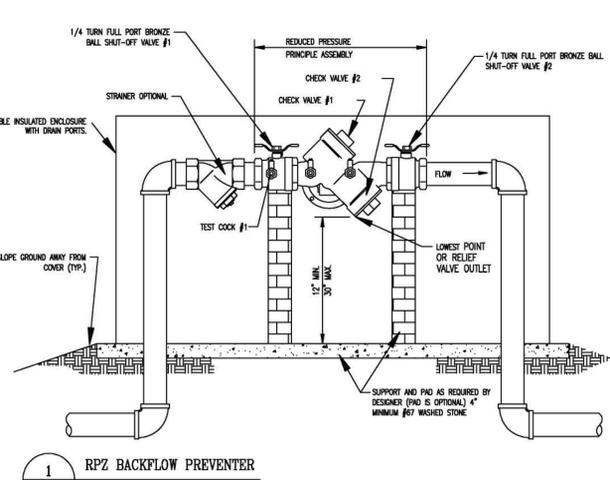
- THERE SHALL BE NO TRAPS, PIPING BRANCHES, UNPROTECTED BY PIPING, HYDRANTS, FIRE DEPARTMENT CONNECTION POINTS OR OTHER WATER-USING APPLIANCES CONNECTED TO THE SUPPLY LINE BETWEEN ANY WATER METER AND IT'S UTILITY DEPARTMENT REQUIRED BACKFLOW PREVENTER.
- EACH UTILITY DEPARTMENT REQUIRED BACKFLOW PREVENTER IS REQUIRED TO BE TESTED BY AN APPROVED CERTIFIED TESTER PRIOR TO PLACING THE WATER SYSTEM INTO SERVICE.
- CONTRACTOR TO COORDINATE WITH HILTON HEAD PUBLIC SERVICE DISTRICT FOR WATER METER & BACKFLOW PREVENTION REQUIREMENTS.



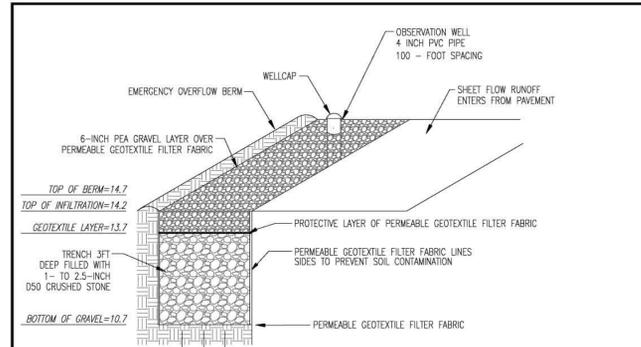
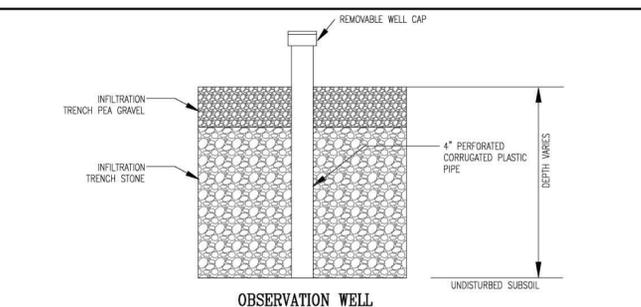
UTILITY CONTACT:

POWER	PALMETTO ELECTRIC COOP. INC. (803) 922-5551
WATER & SEWER	HILTON HEAD PUBLIC SERVICE DISTRICT (843) 681-9525
GAS	SOUTH CAROLINA ELECTRIC & GAS CO. (803) 251-7234

WETLAND NOTE:
NO WETLANDS ARE PROPOSED TO BE DISTURBED BY THE DEVELOPMENT OF THE SITE.



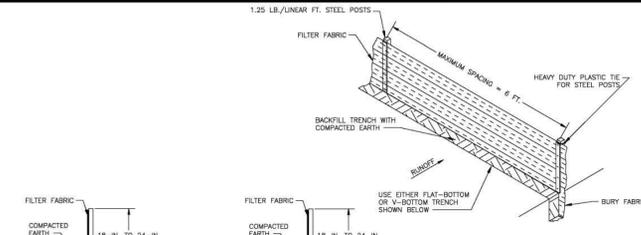
COOPERATIVE WAY



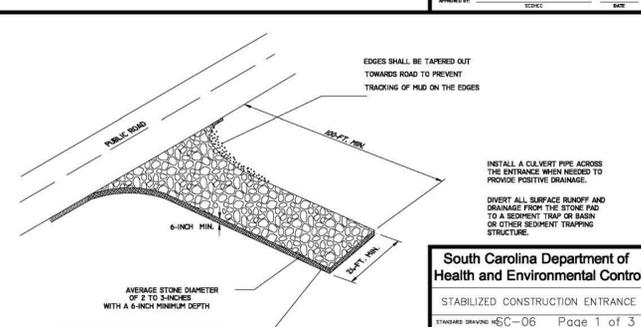
South Carolina Department of Health and Environmental Control
INFILTRATION TRENCH
STANDARD DRAWING NO. WQ-06

INFILTRATION TRENCH
Installation:
The stone fill media shall consist of 1.0- to 2.5-inch D50 crushed stone with 6-inches of pea gravel located on top separated by a permeable filter fabric. This filter fabric prevents should be easily separated from the geotextiles that protect the sides of the excavated trench.
Observation wells with a maximum of 100-ft apart shall be installed in every infiltration trench and shall be made of 4- to 6-inch PVC pipe. The well shall extend to the bottom of the trench. The observation well shall be installed along the centerline of the trench, and be flush with the ground elevation of the trench. The top of the well shall be capped and locked to discourage vandalism and tampering.
Inspection and Maintenance:
Regular inspection and maintenance is critical to the effective operation of infiltration trenches as designed. Maintenance responsibility shall be vested with a responsible authority by means of a legally binding and enforceable maintenance agreement that is executed as a condition of the Storm Water Management Permit approval.
A record shall be kept of the average de-watering time of the infiltration trench to determine if maintenance is required.
The top 6-inch layer of pea gravel and the geotextile separating the pea gravel from the stone media serve as a sediment barrier and will be required to be replaced when full of sediment.
Debris and trash shall be cleared from all inlet and outlet structures monthly.
The observation well shall be checked following 72 hours (3-days) of dry weather after a rainfall event. If complete de-watering is not observed, there may be clogging and proper maintenance shall be performed.
Trees, shrubs, or invasive vegetation shall be removed semi-annually.
If complete failure is observed, total rehabilitation of the trench shall be performed by excavating the trench walls to expose clean soil, and replacing the sand, filter media, gravel, and geotextiles.

South Carolina Department of Health and Environmental Control
INFILTRATION TRENCH
STANDARD DRAWING NO. WQ-06



South Carolina Department of Health and Environmental Control
SILT FENCE
STANDARD DRAWING NO. SC-03 Page 1 of 2



South Carolina Department of Health and Environmental Control
STABILIZED CONSTRUCTION ENTRANCE
STANDARD DRAWING NO. SC-06 Page 1 of 3

APPROVED BY: _____ DATE: 7/30/12

DESIGNER: _____

DATE: 7/30/12

SCALE: 6,845 S.F.

JOB #: 001016 DRAWN BY: CHECK BY: VERSION Q2-06

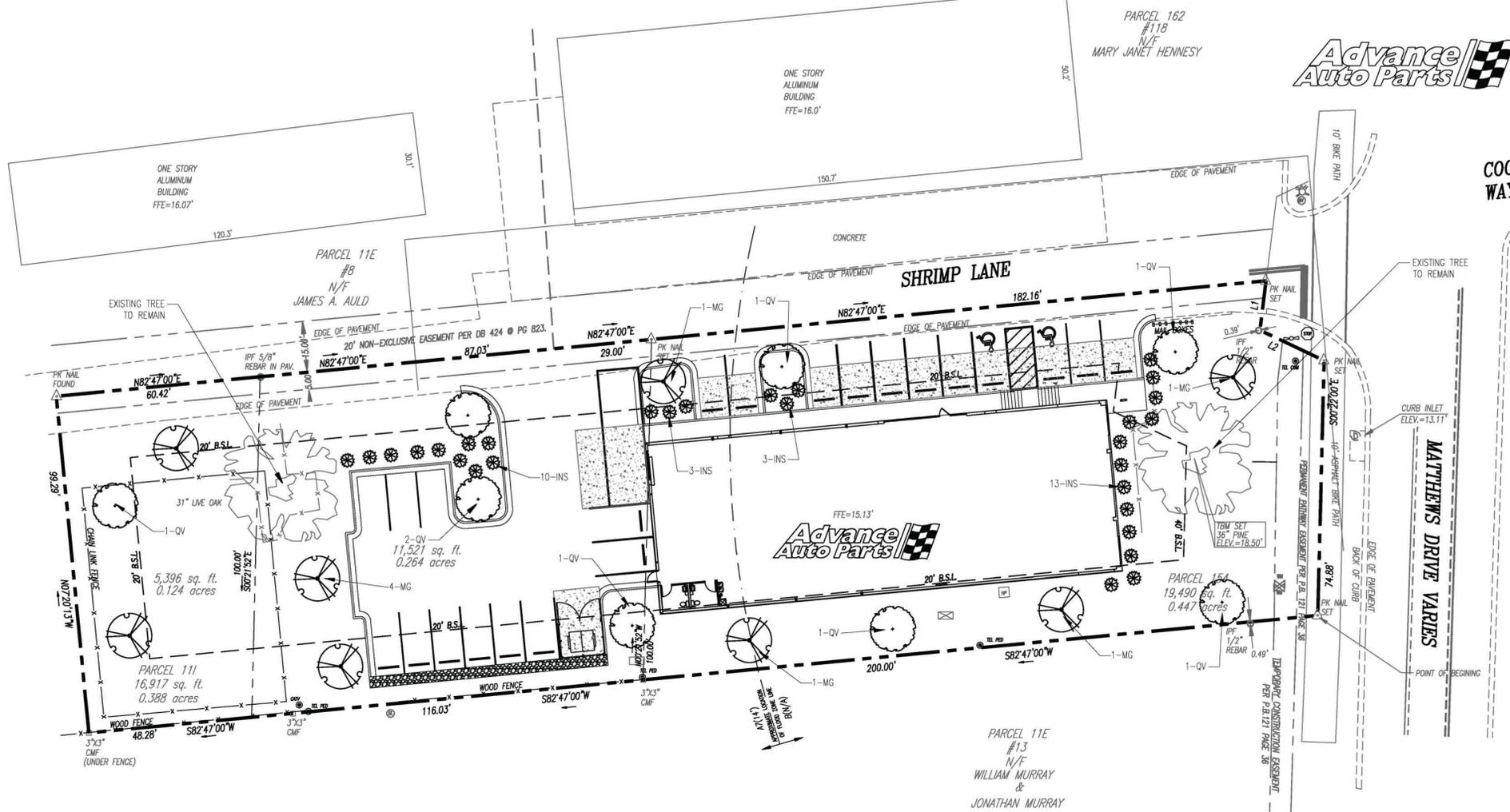
ALL REPORTS, PLANS, SPECIFICATIONS, FIELD DATA AND NOTES AND OTHER DOCUMENTS INCLUDING ALL DOCUMENTS OR ELECTRONIC MEDIA PREPARED BY THE DESIGNER OR ANY SUBSEQUENT REVISIONS SHALL REMAIN THE PROPERTY OF THE DESIGNER. PROFESSIONAL ENGINEERS MAY NOT BE MADE WITHOUT PRIOR CONSENT OF THE DESIGNER. ALL OTHER RIGHTS OF COPYRIGHT AND OTHER RIGHTS ARE HEREBY SPECIFICALLY RESERVED.

South Carolina Professional Engineers Board
No. 21005
7/30/12
NEWBERRY, E. L.

South Carolina Department of Health and Environmental Control
STABILIZED CONSTRUCTION ENTRANCE
STANDARD DRAWING NO. SC-06 Page 1 of 3

C-5

AAP - HILTON HEAD, SC



LANDSCAPE REQUIREMENTS:

TREE COVERAGE REQUIREMENTS:

SITE AREA:	0.84 AC
MAXIMUM IMPERVIOUS AREA:	0.55 AC (65% MAX. IMPERVIOUS)
PERVIOUS AREA:	0.29 AC
ADJUSTED CALIPER INCHES(ACI):	261 CALIPER INCHES REQUIRED
PREDEVELOPMENT ACI:	49 (36" PINE x 0.5 TREE VALUE = 18) (31" LIVE OAK x 1.0 TREE VALUE = 31)
DIFFERENCE BETWEEN REQUIRED & PRE-DEVELOPMENT:	212
15% OF DIFFERENCE:	32 INCHES OF TREES REQUIRED

- TREES PROVIDED:**
- 8-LIVE OAK @ 2IN EACH
 - 8-SOUTHERN MAGNOLIA @ 2IN EACH

TEMPORARY SEEDING SPECIFICATIONS/SCHEDULE

Date	Type	Planting Rate
March - Oct	Browtop Millet	40 lbs/acre
Nov. - Feb.	Winter Rye	120 lbs/acre

SEEDBED PREPARATION:

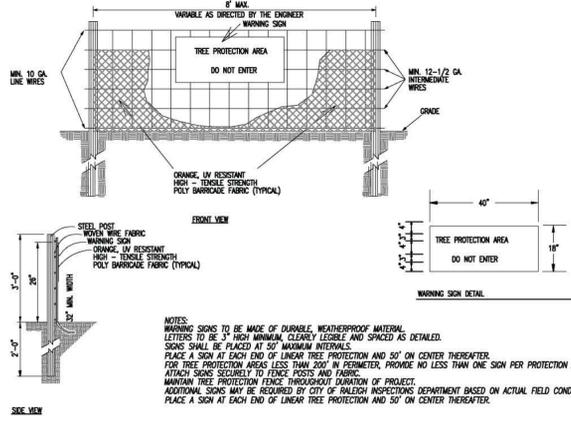
- CHISEL COMPACTED AREAS AND SPREAD TOPSOIL THREE INCHES DEEP OVER ADVERSE SOIL CONDITIONS, IF AVAILABLE.
- RIP THE ENTIRE AREA TO SIX INCHES DEEP.
- REMOVE ALL LOOSE ROCK, ROOTS AND OTHER OBSTRUCTIONS, LEAVING SURFACE REASONABLY SMOOTH AND UNIFORM.
- APPLY AGRICULTURAL LIME, FERTILIZER, AND SUPERPHOSPHATE UNIFORMLY AND MIX WITH SOIL. (SEE SEEDING MIXTURE).
- CONTINUE TILLAGE UNTIL A WELL-PULVERIZED, FIRM, REASONABLY UNIFORM SEEDBED IS PREPARED FOUR TO SIX INCHES DEEP.
- SEED ON A FRESHLY PREPARED SEEDBED AND COVER SEED LIGHTLY WITH SEEDING EQUIPMENT OR OUTRACK AFTER SEEDING.
- MULCH IMMEDIATELY AFTER SEEDING AND ANCHOR MULCH.
- INSPECT ALL SEEDING AREAS AND MAKE NECESSARY REPAIRS OR RESEEDINGS WITHIN THE PLANTING SEASON, IF POSSIBLE. IF STAND SHOULD BE MORE THAN SIX INCHES DAMAGED, RE-ESTABLISH FOLLOWING THE ORIGINAL LIME, FERTILIZER AND SEEDING MIXTURE.
- CONSULT SABC ENVIRONMENTAL ENGINEERS ON MAINTENANCE TREATMENT AND VERIFICATION AFTER PERMANENT COVER IS ESTABLISHED.

SEEDING MIXTURE:

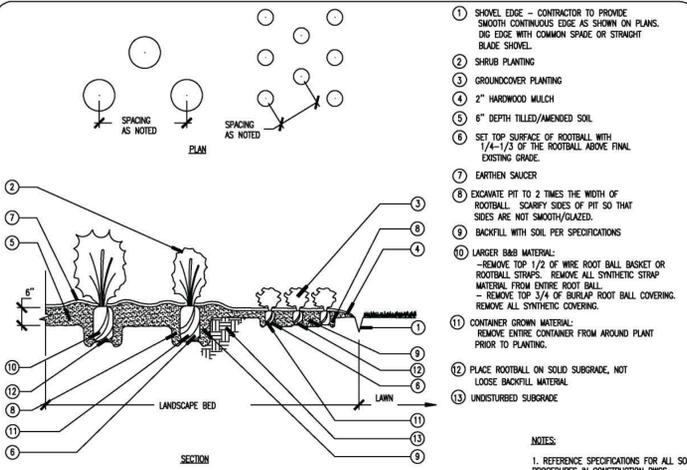
AGRICULTURE LIMESTONE:	2 TONS/ACRE (3 TONS/ACRE IN CLAY SOILS)
FERTILIZER:	1,000 LBS/ACRE - 10-10-10
SUPERPHOSPHATE:	500 LBS/ACRE - 20% ANALYSIS
MULCH:	2 TONS/ACRE - SMALL GRASS STRAW
ANCHOR:	APPLY 1/2" DIAL-SAND AT 300 GALS/ACRE

NOTE 1
Protective cover must be established on all disturbed areas within 21 calendar days after land disturbing activity is completed or has temporarily ceased.

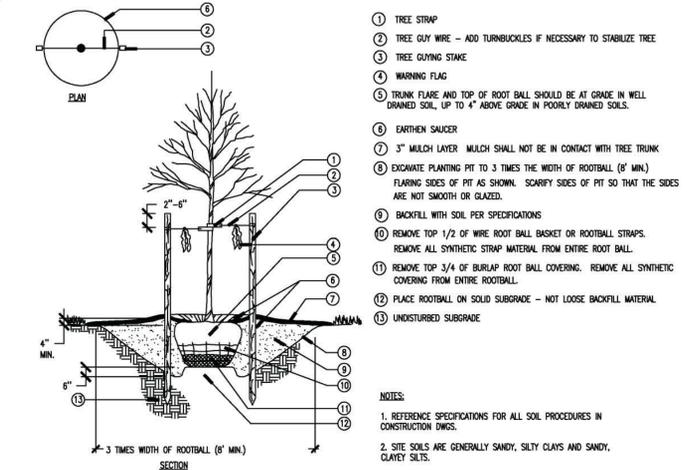
NOTE 2
Graded slopes and fills-- Protective cover must be established on all graded slopes and fills within 21 calendar days after a phase of grading is completed or has temporarily ceased.



3 TREE PROTECTION FENCE
L-1 NOT TO SCALE



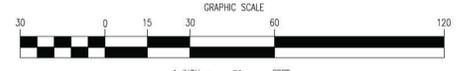
1 TYPICAL LANDSCAPE BED PLANTING DETAIL
(SHRUBS AND GROUND COVER)
L-1 NOT TO SCALE



2 TYPICAL TREE PLANTING/STAKING DETAIL
(FOR 6'-12' TREE HEIGHTS AT PLANTING)
L-1 NOT TO SCALE

LANDSCAPING NOTES:

- CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLETING ALL REQUIRED LANDSCAPING AND IRRIGATION FOR THE ENTIRE SITE, TO INCLUDE BUT NOT LIMITED TO: SOGGED AREAS, SHRUB BEDS, PARKING LOT ISLANDS, ROADSIDE SIGN BASE(S) AND MONUMENT PLANTERS.
- CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE CODES AND ORDINANCES REGARDING LANDSCAPING.
- IRRIGATION CONTRACTOR SHALL BE EXPERIENCED IN IRRIGATION DESIGN AND INSTALLATION AND SHALL PROVIDE PROOF OF CERTIFICATION AS A "CERTIFIED IRRIGATION CONTRACTOR" ACCORDING TO THE IRRIGATION ASSOCIATION OF AMERICA. CONTRACTOR SHALL PROVIDE AN IRRIGATION SYSTEM INSTALLATION WITH 100% COVERAGE OF DESIGNATED PLANTING AREAS USING HEAD TO HEAD COVERAGE, MINIMIZING POSSIBLE OVERTHROW ONTO NON-POROUS SURFACES. IRRIGATION SYSTEM SHALL BE ZONED AND TIMED AS APPROPRIATE TO MEET PLANT MATERIAL AND LAWN AREA WATERING REQUIREMENTS. TIMER/CONTROL TO BE LOCATED INSIDE BUILDING NEAR ELECTRICAL PANEL.
- IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ESTABLISH A HEALTHY STAND OF GRASS ON ALL SEEDING AREAS.
- IN THE EVENT THAT PLANTING BEDS AND MULCH ARE REQUIRED, THE CONTRACTOR SHALL INSTALL BLACK FABRIC WEED BLOCK LANDSCAPE MESH UNDER THE MULCH TO PREVENT WEED GROWTH.
- CONTRACTOR SHALL PROVIDE NATURAL TOPSOIL THAT IS FERTILE, FRIABLE, WITHOUT MIXTURE OF SUBSOIL MATERIALS, AND OBTAINED FROM A WELL DRAINED, AVAILABLE SITE. IT SHALL NOT CONTAIN SUBSTANCES WHICH MAY BE HARMFUL TO PLANT GROWTH. TOPSOIL SHALL BE SCREENED AND FREE FROM CLAY, LUMPS, STONES, ROOTS, PLANTS, OR SIMILAR SUBSTANCES 1" OR MORE IN DIAMETER, DEBRIS, OR OTHER OBJECTS WHICH MIGHT BE A HINDRANCE TO PLANTING OPERATIONS. TOPSOIL SHALL CONTAIN AT LEAST 4-6% ORGANIC MATTER BY WEIGHT AND HAVE A PH RANGE OF 5.5 TO 7.0 OR AS APPLICABLE TO THE REGION.
- CONTRACTOR SHALL BE RESPONSIBLE FOR THE WATERING AND MAINTENANCE OF ALL LANDSCAPED AREAS UNTIL THE LATER OF: (a) THIRTY (30) DAYS FOLLOWING THE PLANTING OF THE GRASS AND SHRUBS, OR (b) THE DATE THAT ADVANCE AUTO PARTS OPENS FOR BUSINESS TO THE PUBLIC. CONTRACTOR TO WARRANT ALL LANDSCAPING FOR A TOTAL OF 1 YEAR.
- GENERAL CONTRACTOR IS TO CLEAN ENTIRE SITE OF ALL CONSTRUCTION DEBRIS AND RAKE ALL GRASS AREAS. GRASS (500) TO BE LEVEL, ROLLED AND MOWABLE.
- PROVIDE LANDSCAPE PLANS TO ADVANCE AUTO PARTS AND AS REQUIRED BY LOCAL JURISDICTION TO THE BLDG. DEPT. FOR REVIEW AND APPROVAL PRIOR TO START OF WORK.
- ALL LANDSCAPING, TREES, SHRUBS, ETC. SHALL NOT INTERFERE WITH THE VISIBILITY OF ACP MONUMENT SIGNAGE.
- CONTRACTOR TO VERIFY QUANTITIES PRIOR TO COMMENCING WORK.
- ALL PLANTING AREAS WITHIN PROPERTY BOUNDARY ARE TO BE WATERED WITH A FULL AUTOMATIC UNDERGROUND SPRINKLE SYSTEM WITH FREEZE GUARD. ALL IRRIGATION COMPONENTS SHALL BE CONTAINED WITHIN THE BOUNDARY OF THE SITE. IRRIGATION TO PLANTING AREAS OUTSIDE OF THE PROPERTY BOUNDARY SHALL ORIGINATE FROM IRRIGATION HEADS WITHIN THE PROPERTY BOUNDARY. CONTRACTOR SHALL RETAIN THE SERVICES OF A LICENSED IRRIGATOR WHO SHALL PROVIDE DETAILED IRRIGATION DRAWINGS WITH SUPPORTING PRESSURE LOSS AND FLOW CALCULATIONS. THESE SHALL BE SUBMITTED TO LANDSCAPE ARCHITECT AS SHOP DRAWINGS FOR REVIEW AND APPROVAL PRIOR TO COMMENCING WORK.
- OPEN AREAS WITHIN PLANTING BEDS SHALL BE MULCHED WITH HARDWOOD ONLY. PINE STRAW OR EQUIVALENT IS NOT AN ACCEPTABLE MULCH.



PLANT LIST

QTY	KEY	PLANT NAME	SIZE (MIN)	ROOT	REMARKS
8	OV	LIVE OAK/Quercus Virginiana	2" CAL.	B&B	10' HEIGHT
8	MG	SOUTHERN MAGNOLIA/Magnolia grandiflora	2" CAL.	B&B	10' HEIGHT
29	INS	NELLY STEVENS HOLLY/Ilex X 'NELLY STEVENS'	1 1/2" CAL.	B&B	8' HEIGHT

TRIANGLE SITE DESIGN, PLLC
4006 Barrett Drive
Suite 203
Raleigh, NC 27609
(919)559-6570

HARTZOG HOLDINGS, LLC
109 STILL WILD LANE
ELGIN, SC 29045
803-361-8719
collins@hartzogholdings.com

Advance Auto Parts
200 MATTHEWS DRIVE
TOWN OF HILTON HEAD
BEAUFORT COUNTY, SC

REVISIONS

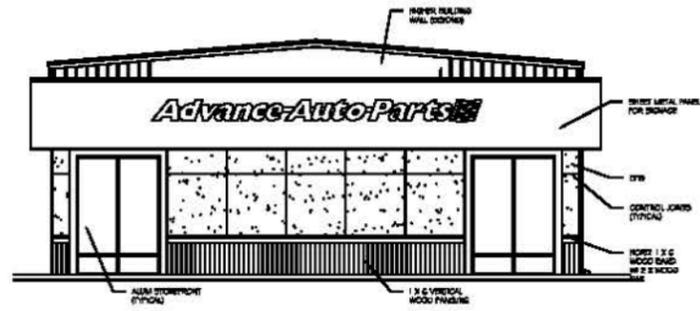
NO.	DATE	BY	DESCRIPTION
1			
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6			

LANDSCAPE PLAN

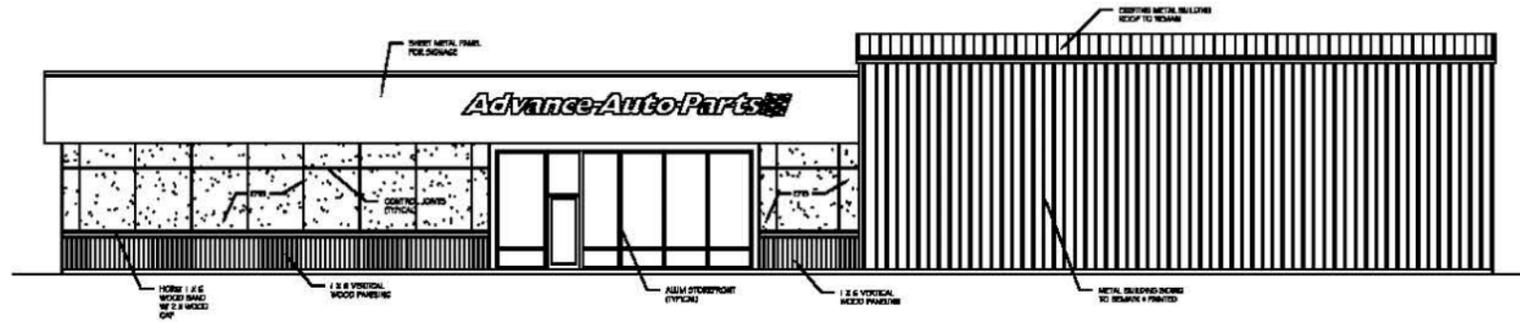
DATE: 7/30/12
JOB # 0016
DRAWN BY: CEEBCK BYT

VERSION Q2-08
ALL REPORTS, PLANS, SPECIFICATIONS, FIELD DATA AND NOTES AND OTHER DOCUMENTS INCLUDING ALL DOCUMENTS ON ELECTRONIC MEDIA, PREPARED BY THE DESIGN PROFESSIONAL AS INSTRUMENTS OF SERVICE SHALL REMAIN THE PROPERTY OF THIS DESIGN PROFESSIONAL. REDEMPTION MAY NOT BE MADE WITHOUT PRIOR CONSENT OF THE DESIGN PROFESSIONAL. ALL COMMON LAW RIGHTS OF COPYRIGHT AND INVENTION ARE HEREBY SPECIFICALLY RESERVED.

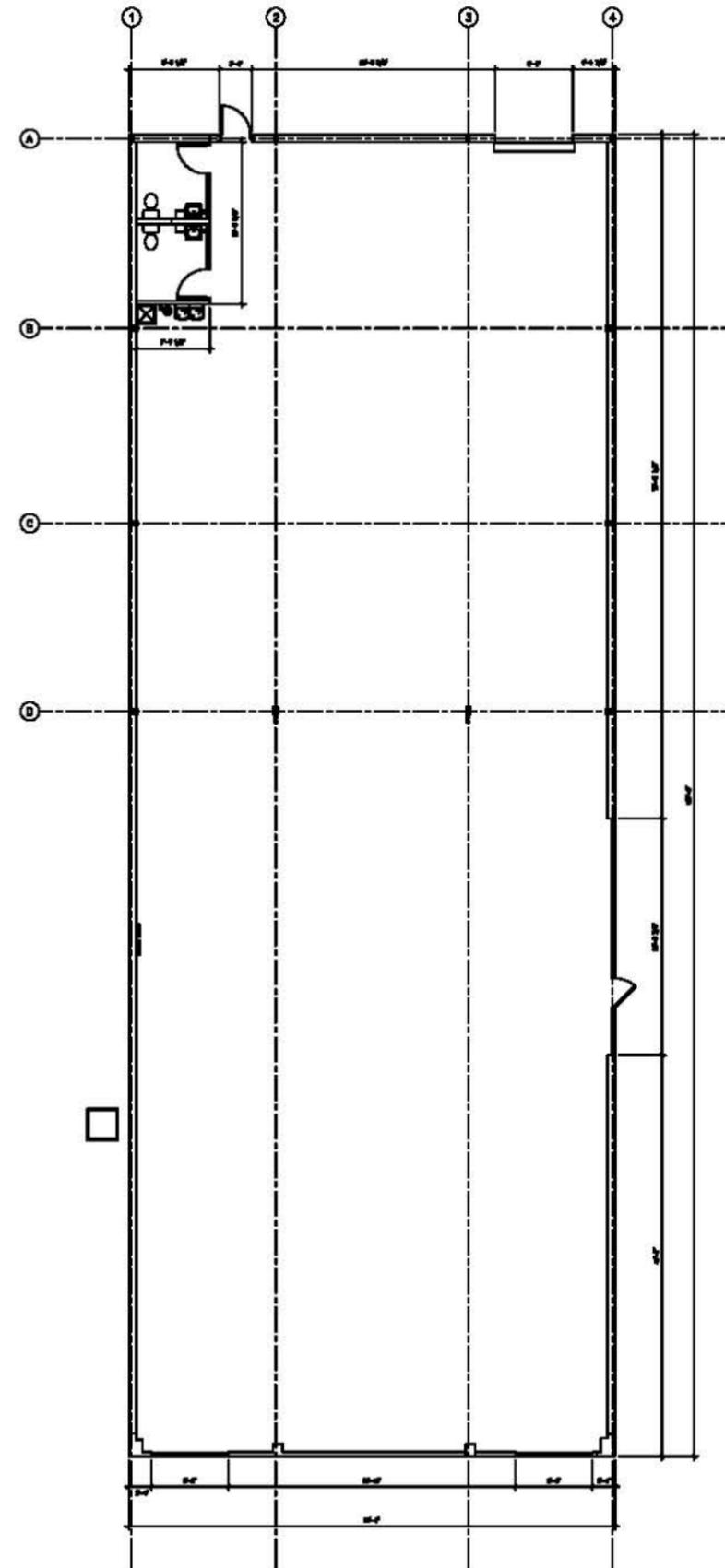
Professional Seal
SOUTH CAROLINA PROFESSIONAL ENGINEER
No. 21005
7/30/12
Matthew Hand



1 FRONT ELEVATION
SCALE: 1/8" = 1'-0"



2 RIGHT SIDE ELEVATION
SCALE: 1/8" = 1'-0"



3 FLOOR PLAN
SCALE: 1/8" = 1'-0"



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WARNER ARCHITECTURAL ASSOCIATES
109 ANDERSON STREET SE. SUITE 105
MARIETTA, GA 30060
TEL: 678.290.9200 / FAX: 678.290.9250
SC FIRM# ARF 100712



Advance Auto Parts
120 MATTHEWS DR
HILTON HEAD, SC 29926

Drawn by:	ROW
Checked by:	WVW
Date:	7.27.2012
REVISIONS	
Date:	
Working Number:	A1



1 FRONT ELEVATION
A01 SCALE: 1/8" = 1'-0"



2 RIGHT SIDE ELEVATION
A01 SCALE: 1/8" = 1'-0"

GLC20

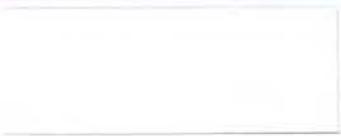
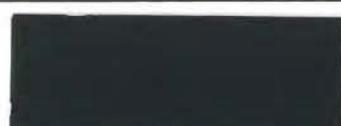
Natural Linen™

EFIS

GLN04

Mossy Green™

WOOD
SIDING

Finish Code	Application	Specification	Color Name, Numbers and Color Sample
IP1	Interior Drywall, Interior Block Walls, Interior Metal Columns & Rear Doors	Primer - See Spec Finish - 1412V	AAP-INT1 White High-Hiding RM Order #A0113 MP#98YY 82/022 
IP2	Bathrooms Doors & Trim	Primer - 3210 Finish - 1416V	AAP-INT2 Custom Interior Color Gallon Formula: BLK1P34, YOX44, OXR20 
IP3	Ceiling Decks	Primer - See Spec Finish - 1280	AAP-INT3 Universal Grey Order #A2004 MP#00NN 62/000 
A1	Exterior Block Walls, Stucco, Metal & Back Door Finishes	Primer - See Spec Ext. Finish - 2402V Door Finish - 4216HP	AAP-A1 Beachcomber Order #A1788 MP#20YY 58/082 
B1	Exterior Block Walls, Stucco, Metal & Back Door Finishes	Primer - See Spec Ext. Finish - 2402V Door Finish - 4216HP	AAP-B1 Castle Rock Order #A1776 MP#10YY 41/083 
B2	Exterior Block Walls, Stucco, Metal & Back Door Finishes	Primer - See Spec Ext. Finish - 2402V Door Finish - 4216HP	AAP-B2 Sand Motif Order #A1748 MP#00YY 38/123 
R1	Exterior CMU/Smooth Board (Prototype)	Primer - 3030 Finish - 3006-8500	AAP-R1 Advance Auto Parts Red Gallon Formula: MAG24, FFR8P8, WHT1P32 
R2	Exterior Metal & Metal Door Finishes	Primer - 201/203/205 Finish - 379B9502 Top Coat - 379H0036	AAP-R2 Advance Auto Parts Red Gallon Formula: QR3P32, UO6P, TW32 
	Exterior Metal Railings	Primer - 4160 Finish - 4216-9990L	Black 
	Bollards, Safety Requirements	Primer - 4160 Finish - 4216-9400L	Safety Yellow 

* For national account support, please call (888) 615-8169 option 2. Please consult the latest Advance Auto Parts specification or Glidden Professional National Accounts for specific finish schedule requirements.

* For technical reasons to do with color reproduction, all colors on this card are a representation of the actual paint color only. Please refer to actual paint chip to ensure color accuracy. 11.CM.00058 March, 2011

DESIGN TEAM/DRB COMMENT SHEET

*The comments below are staff recommendations to the Design Review Board (DRB)
and do NOT constitute DRB approval or denial.*

PROJECT NAME: Advance Auto Parts – ALTERATION/ADDITION

DRB#: DR 120022

DATE: August 14, 2012

RECOMMENDATION: Approval Approval with Conditions Denial

<i>ARCHITECTURAL DESIGN</i>				
DESIGN GUIDE/LMO CRITERIA	Complies Yes	No	Not Applicable	Comments or Conditions
Structure is designed to be appropriate to the neighborhood	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Promotes pedestrian scale and circulation	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Design is unobtrusive and set into the natural environment	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Utilizes natural materials and colors	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Avoids distinctive vernacular styles	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Design is appropriate for its use	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
All facades are have equal design characteristics	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
Avoids monotonous planes or unrelieved repetition	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
Has a strong roof form with enough variety to provide visual interest	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
Minimum roof pitch of 6/12	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Overhangs are sufficient for the façade height.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Forms an details are sufficient to reduce the mass of the structure	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Human scale is achieved by the use of proper proportions and architectural elements	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	

Utilizes a variety of materials, textures and colors	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Incorporates wood or wood simulating materials	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Windows are in proportion to the facade	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Details are clean, simple and appropriate while avoiding excessive ornamentation	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Utilities and equipment are concealed from view	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Decorative lighting is limited and low wattage and adds to the visual character	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Accessory elements are design to coordinate with the primary structure	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	

LANDSCAPE DESIGN				
DESIGN GUIDE/LMO CRITERIA	Complies Yes	No	Not Applicable	Comments or Conditions
Treats the Landscape as a major element of the project	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Provides Landscaping of a scope and size that is in proportion to the scale of the development	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Landscape is designed so that it may be maintained in its natural shape and size	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Preserves a variety of existing native trees and shrubs	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Provides for a harmonious setting for the site's structures, parking areas or other construction	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Location of existing trees and new trees provides street buffers, mitigation for parking lots, and an architectural complement that visually mitigates between parking lots and building(s)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Shrubs are selected to complement the natural setting, provide visual interest and screen less desirable elements of the project	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Proposed shrubs will screen side of building from Matthews Drive and rear parking lot from Shrimp Lane. Smaller shrub should be considered for planting in parking lot islands.
A variety of species is selected for texture and color	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
Provides overall order and continuity of the Landscape plan	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Native plants or plants that have historically been prevalent on the Island are utilized	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
A variety of sizes is selected to create a "layered" appearance for visual interest and a sense of depth	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
The location of existing mature trees is taken into account in placement of shrubs so as not to damage	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	

tree roots				
Proper spacing and location for plants to reach their mature size and natural shape while avoiding excessive or unnatural pruning	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
Proposed groundcovers are evergreen species with low maintenance needs	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Large grassed lawn areas encompassing a major portion of the site are avoided	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
The adjacent development is taken into account in determining the most appropriate buffer so as not to depart too dramatically from the neighborhood	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Ornamentals and Annuals are limited to entrances and other focal points	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	

NATURAL RESOURCE PROTECTION

DESIGN GUIDE/LMO CRITERIA	Complies Yes	No	Not Applicable	Comments or Conditions
An effort has been made to preserve existing trees and under story plants	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Supplemental and replacement trees meet LMO requirements for size, species and number	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Wetlands if present are avoided and the required buffers are maintained	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Sand dunes if present are not disturbed	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	

MISC COMMENTS/QUESTIONS

Applicant has made efforts to apply Design Guide principals to existing large metal building. Staff recommends standard “red” be toned down to meet intent of Design Guide.



Town of Hilton Head Island
 Community Development Department
 One Town Center Court
 Hilton Head Island, SC 29928
 Phone: 843-341-4757 Fax: 843-842-8908
www.hiltonheadislandsc.gov

FOR OFFICIAL USE ONLY
 Date Received: 7/31/12
 Accepted by: DH
 App. #: DR 120023
 Meeting Date: 8/14/12

Applicant/Agent Name: DALE JOHNSON Company: THE JOHNSON PARTNERSHIP
 Mailing Address: 32 OFFICE PARK RD. #104 City: H.H.I State: SC Zip: 29928
 Telephone: 785-4666 Fax: - E-mail: drjarchitect@aol.com
 Project Name: SKULL CREEK BATHHOUSE Project Address: 397 Squire Pope Rd
 Parcel Number [PIN]: R
 Zoning District: _____ Overlay District(s): DRB

DESIGN REVIEW BOARD (DR) SUBMITTAL REQUIREMENTS

Digital Submissions may be accepted via e-mail by calling 843-341-4757.

Project Category:

- New Development – Conceptual
- New Development – Final, indicate Project Number
- Alteration/Addition
- Minor External Change

Submittal Requirements for *All* projects:

N/A Architectural Review Board (ARB) Notice of Action (if applicable): When a project is within the jurisdiction of an ARB, the applicant shall submit such ARB's written notice of action per LMO Section 16-3-1004. Submitting an application to the ARB to meet this requirement is the responsibility of the applicant.

Filing Fee, New Development \$175, Alterations/Additions \$100, Minor External \$50 cash or check made payable to the Town of Hilton Head Island.

Additional Submittal Requirements:

New Development – Conceptual Approval

- A survey (1"=30' minimum scale) of property lines, existing topography and the location of trees meeting the tree protection regulations of Sec. 16-3-405, and if applicable, location of bordering streets, marshes and beaches.
- A site analysis study to include specimen trees, access, significant topography, wetlands, buffers, setbacks, views, orientation and other site features that may influence design.
- A draft written narrative describing the design intent of the project, its goals and objectives and how it reflects the site analysis results.
- Context photographs of neighboring uses and architectural styles.
- Conceptual site plan (to scale) showing proposed location of new structures, parking areas and landscaping.
- Conceptual sketches of primary exterior elevations showing architectural character of the proposed development, materials, colors, shadow lines and landscaping.

Additional Submittal Requirements:

New Development – Final Approval

- _____ A final written narrative describing how the project conforms with the conceptual approval and design review guidelines of Sec. 16-4-503.
- _____ Final site development plan meeting the requirements of Sec. 16-3-303.F.
- _____ Final site lighting and landscaping plans meeting the requirements of Sec. 16-3-304 and Sec. 16-3-305.
- _____ Final floor plans and elevation drawings (1/8"=1'-0" minimum scale) showing exterior building materials and colors with architectural sections and details to adequately describe the project.
- _____ A color board (11"x17" maximum) containing actual color samples of all exterior finishes, keyed to the elevations, and indicating the manufacturer's name and color designation.
- _____ Any additional information requested by the Design Review Board at the time of concept approval, such as scale model or color renderings, that the Board finds necessary in order to act on a final application.

Additional Submittal Requirements:

Alterations/Additions *and* Minor External Changes

- _____ A written narrative describing how project conforms to design guidelines of Section 16-4-503.
- _____ Photographs and/or drawings of existing development.
- _____ Drawings of the proposed development – 11"x 17".
- _____ Material/color samples of existing and proposed changes - 8 1/2"X 14" *Maximum*; Stating manufacturer and material name

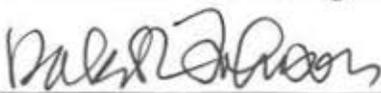
Note: All application items must be received by the deadline date in order to be reviewed by the DRB per LMO Section 16-3-106.

A representative for each agenda item is strongly encouraged to attend the meeting.

Are there recorded private covenants and/or restrictions that are contrary to, conflict with, or prohibit the proposed request? If yes, a copy of the private covenants and/or restrictions must be submitted with this application. YES NO

To the best of my knowledge, the information on this application and all additional documentation is true, factual, and complete. I hereby agree to abide by all conditions of any approvals granted by the Town of Hilton Head Island. I understand that such conditions shall apply to the subject property only and are a right or obligation transferable by sale.

I further understand that in the event of a State of Emergency due to a Disaster, the review and approval times set forth in the Land Management Ordinance may be suspended.



SIGNATURE

31 July 12

DATE

NARRATIVE IN SUPPORT OF MODIFICATIONS TO

SKULL CREEK BOATHOUSE

The corner of the boathouse building closest to the Boathouse Restaurant currently houses a Boat Store with an interior entrance. Applicant desires to convert that space into two retail uses. One will remain a boat store and the other will be a retail annex to the restaurant. Both uses will feature exterior entries. Two new toilet rooms will be added to serve the retail and restaurant spaces.

Since the boat storage portion of the building has a floor elevation of +12 msl and the retail portion requires +14 msl, an exterior stair and ramp must be constructed. They and the entrance deck will be constructed of treated wood. Stair, deck and ramp picketing will be a continuation of existing material. An awning matching the one at the restaurant entrance will be included. No other improvements are contemplated.



Copyright ©2011 Pictometry International Corp.

Creation Date: Feb 11, 2011 11:38
Modification Date: Mar 22, 2011 10:05

Average Scale: 1 inch = 30.8 feet

Distance between tick marks: 29.79 feet





Lat. N

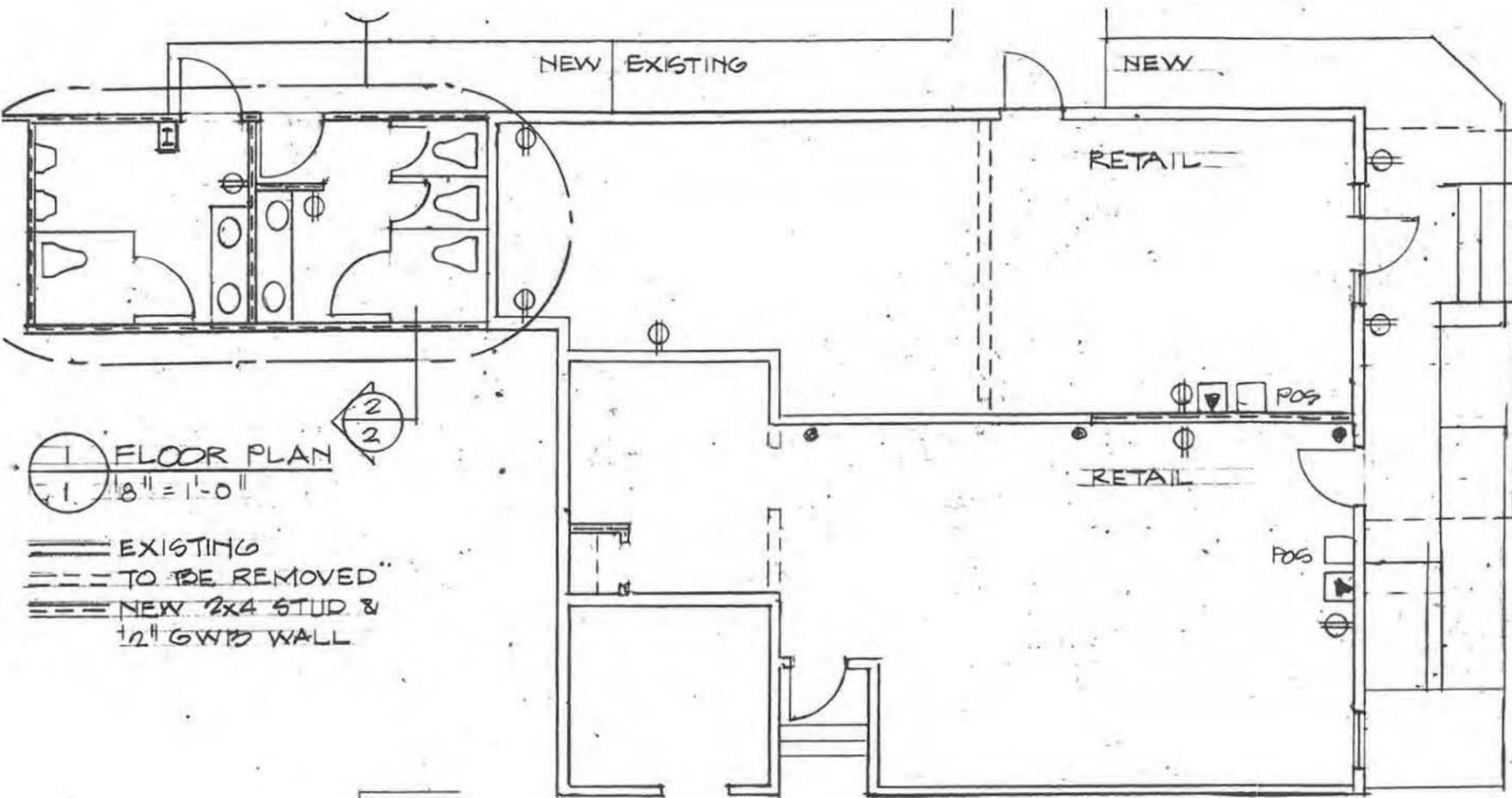
PIPER CREEK BOATHOUSE ★

500

RESTAURANT

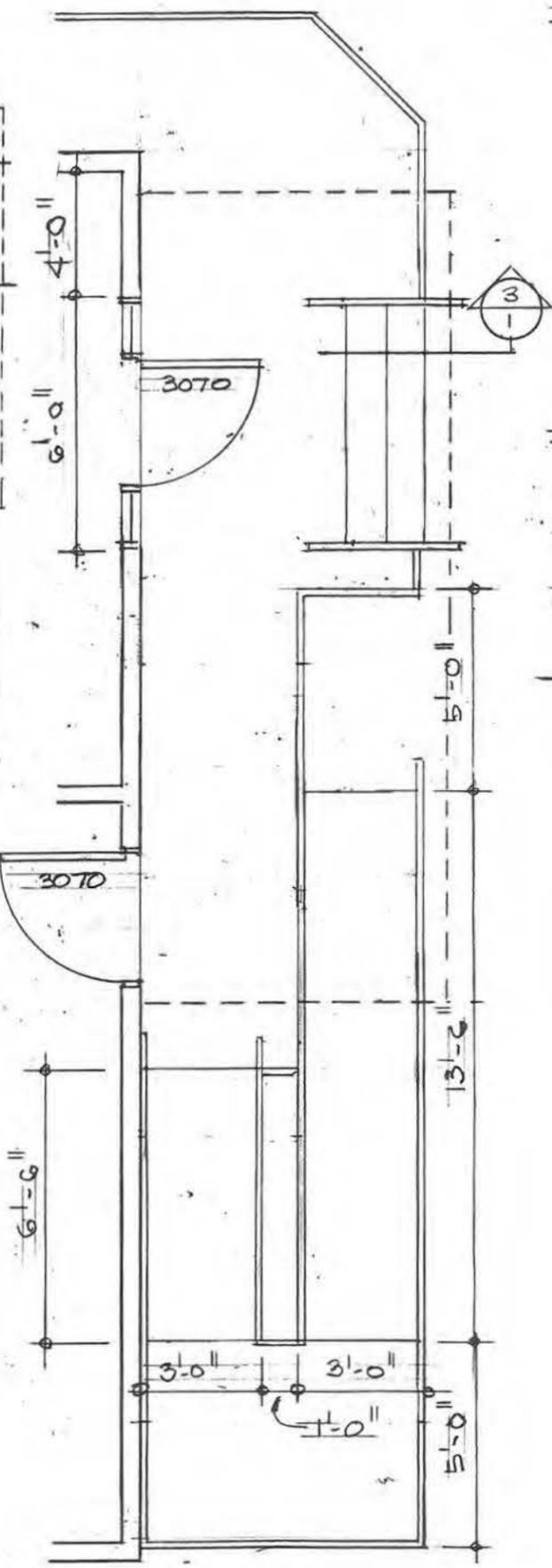
BAR



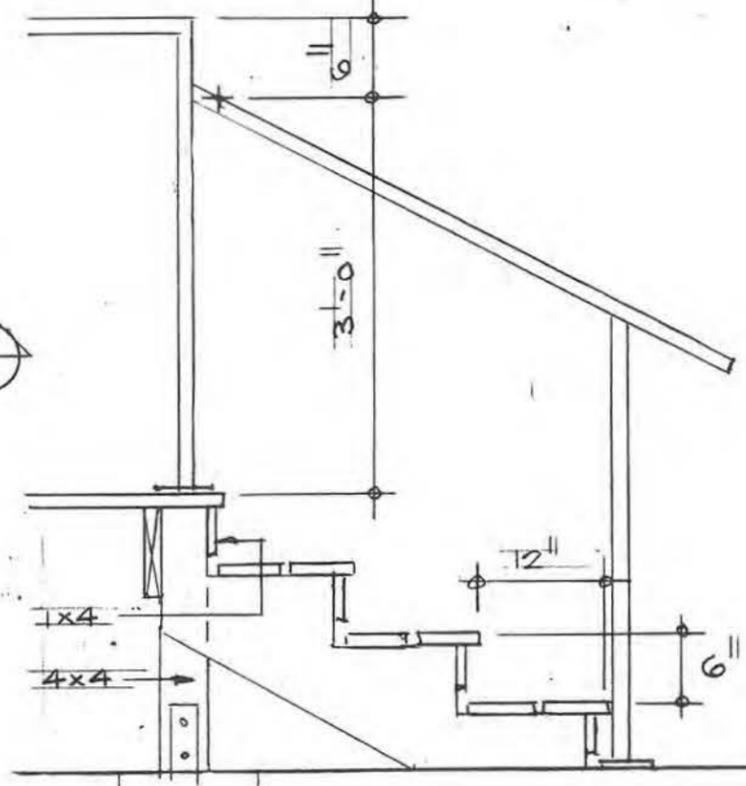


1 FLOOR PLAN
1/8" = 1'-0"

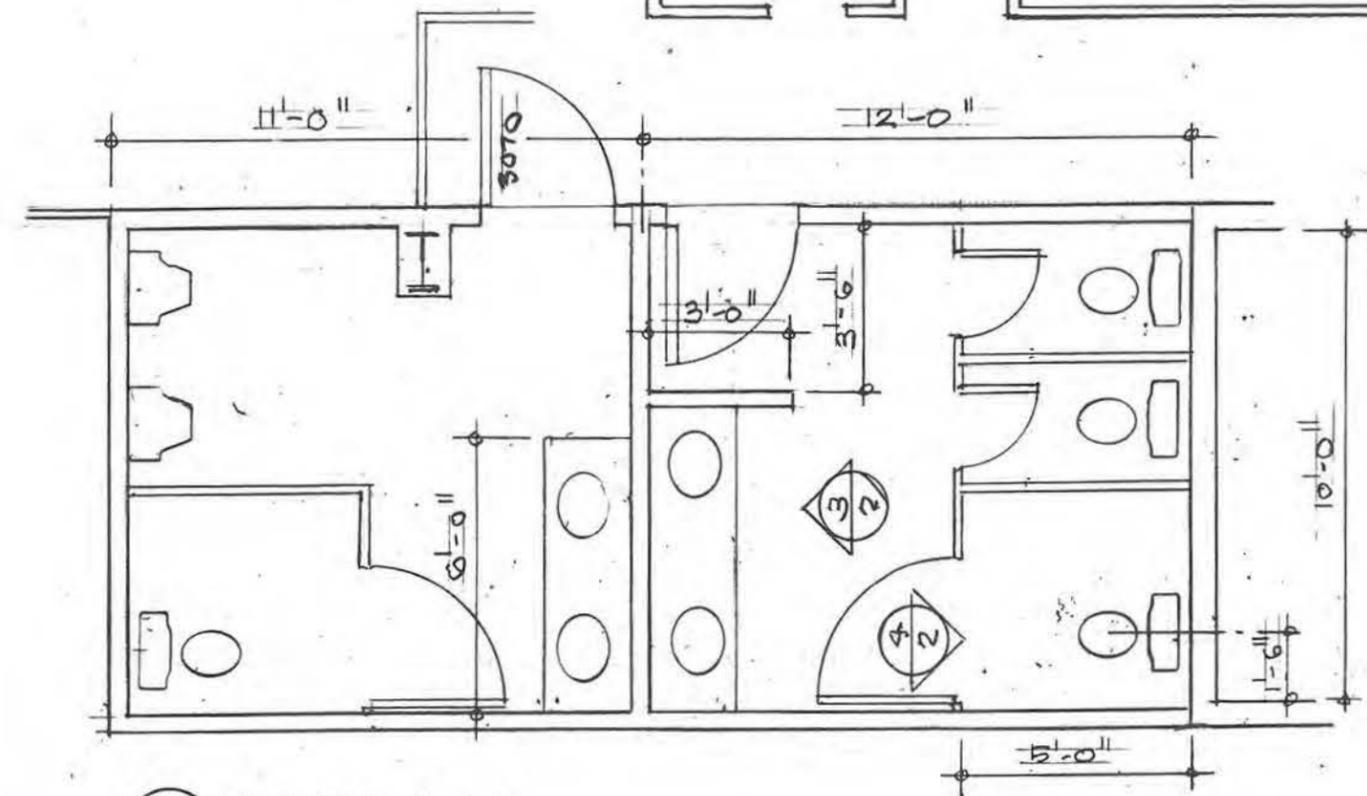
EXISTING
TO BE REMOVED
NEW 2x4 STUD & 1/2" GWB WALL



3 PORCH
1/4" = 1'-0"



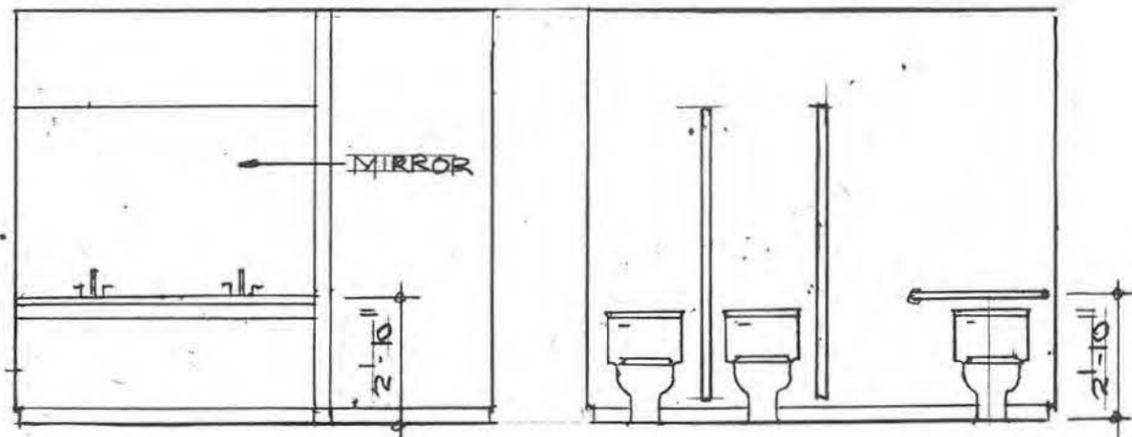
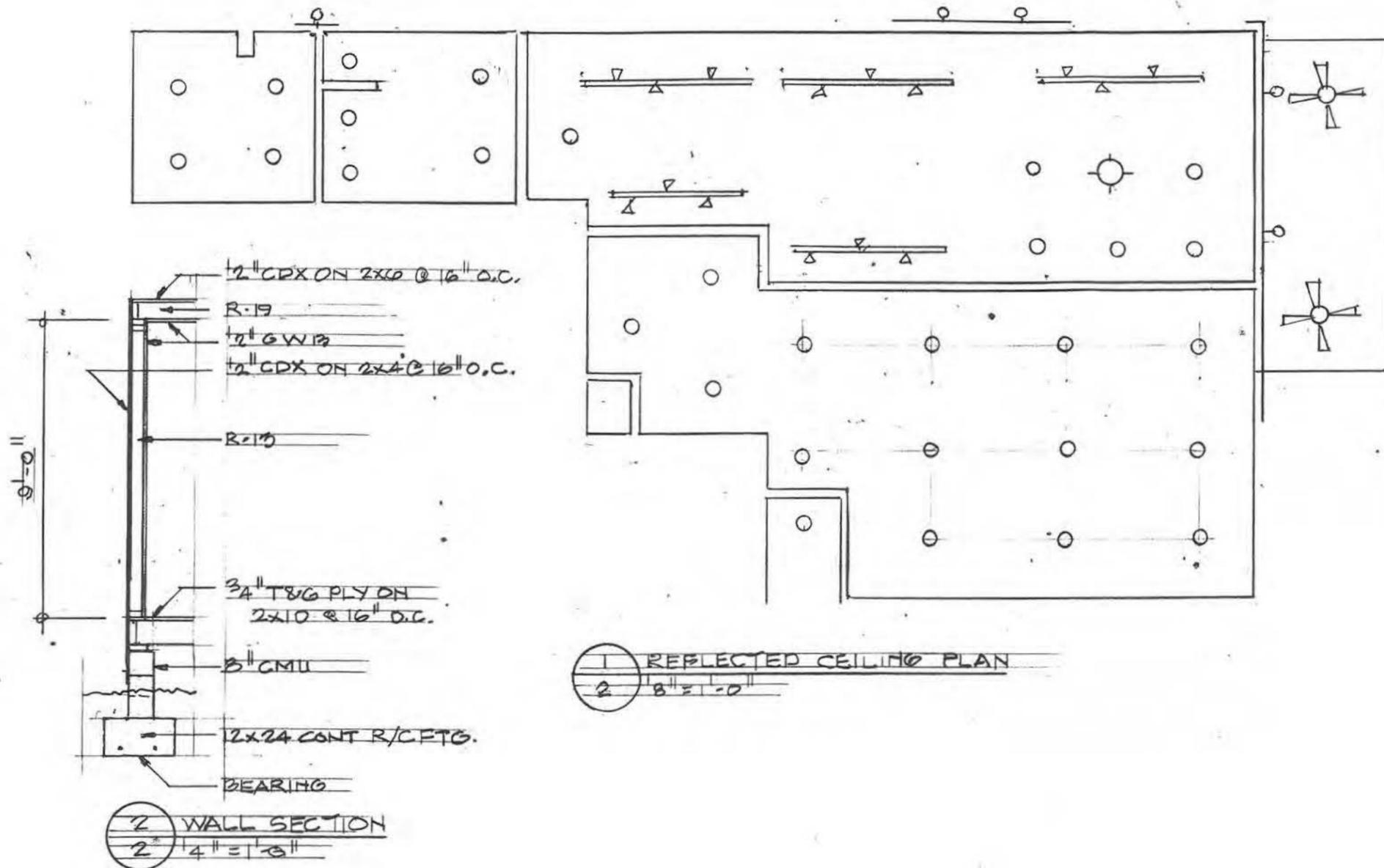
STAIR PICKETS TO MATCH EXIST'G.
STRINGERS FROM 2x12 @ 16" O.C.
TREADS: 2~54x6 @ 5 3/4" O.C.
STRAPS: ALPINE WAP 44
DECK: 54x6 @ 5 3/4" O.C.

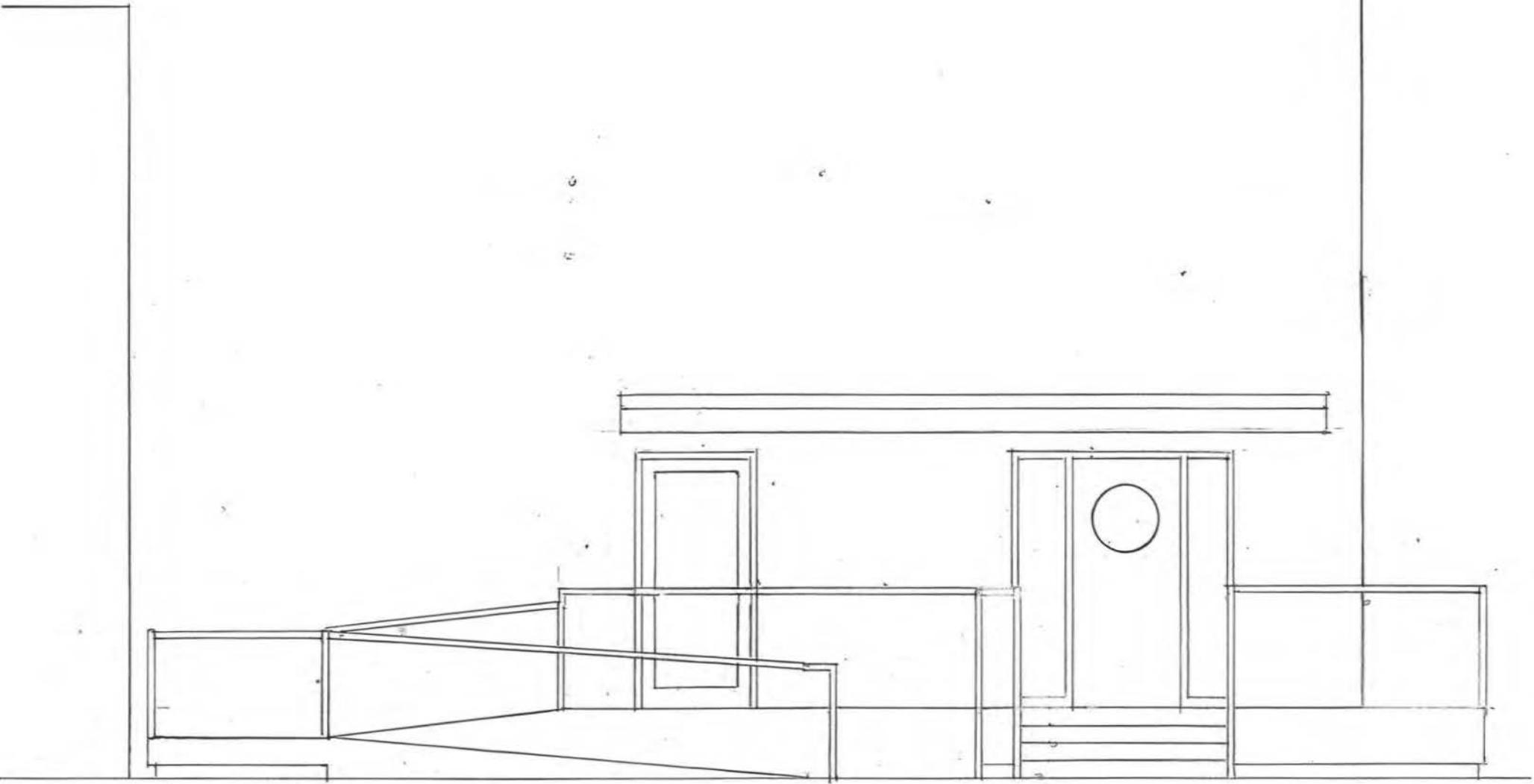


2 TOILETS PLAN
1/4" = 1'-0"

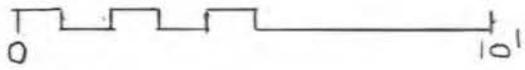
VERIFY ALL DIMENSIONS

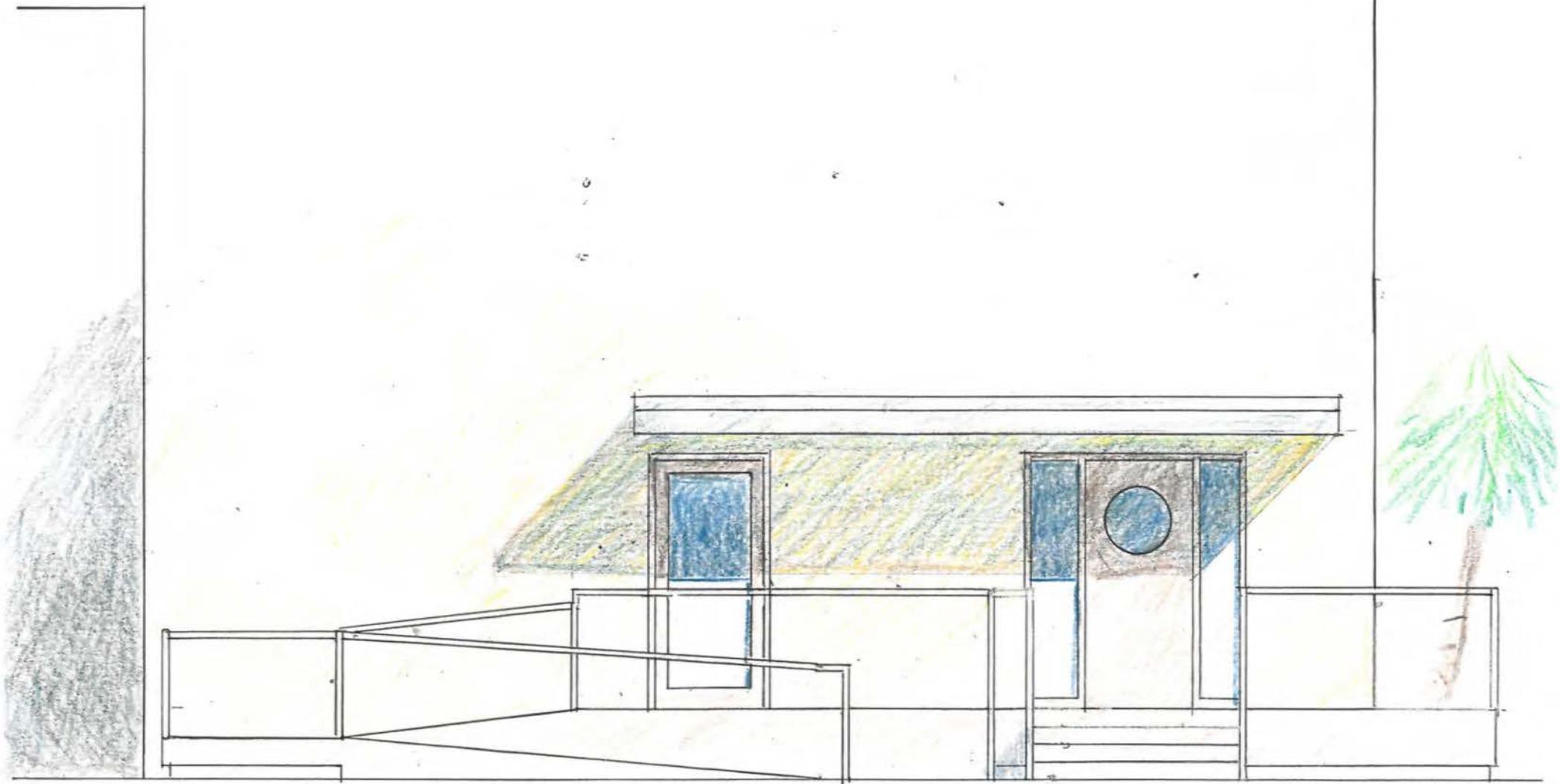
Johnson Partnership ARCHITECTS
104 The Courtyard Building
Wilton Head Island, SC 29928



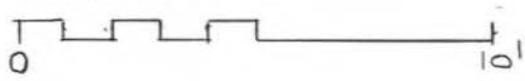


ELEVATION





ELEVATION



DESIGN TEAM/DRB COMMENT SHEET

*The comments below are staff recommendations to the Design Review Board (DRB)
and do NOT constitute DRB approval or denial.*

PROJECT NAME: Skull Creek Boathouse – ALTERATION/ADDITION

DRB#: DR 120023

DATE: August 14, 2012

RECOMMENDATION: Approval Approval with Conditions Denial

<i>ARCHITECTURAL DESIGN</i>				
DESIGN GUIDE/LMO CRITERIA	Complies Yes	No	Not Applicable	Comments or Conditions
Structure is designed to be appropriate to the neighborhood	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Promotes pedestrian scale and circulation	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Design is unobtrusive and set into the natural environment	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Utilizes natural materials and colors	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Materials and colors are proposed to match adjacent restaurant.
Avoids distinctive vernacular styles	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Design is appropriate for its use	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
All facades are have equal design characteristics	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Avoids monotonous planes or unrelieved repetition	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Has a strong roof form with enough variety to provide visual interest	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Minimum roof pitch of 6/12	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Overhangs are sufficient for the façade height.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Forms an details are sufficient to reduce the mass of the structure	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Human scale is achieved by the use of proper proportions	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	

and architectural elements				
Utilizes a variety of materials, textures and colors	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Incorporates wood or wood simulating materials	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Windows are in proportion to the facade	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Details are clean, simple and appropriate while avoiding excessive ornamentation	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	While not indicated on the plans, intent is to match details at restaurant entrance.
Utilities and equipment are concealed from view	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Decorative lighting is limited and low wattage and adds to the visual character	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	None shown on plans. Any proposed lighting should match existing lighting at restaurant entrance.
Accessory elements are design to coordinate with the primary structure	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	

LANDSCAPE DESIGN

DESIGN GUIDE/LMO CRITERIA	Complies Yes	No	Not Applicable	Comments or Conditions
Treats the Landscape as a major element of the project	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
Provides Landscaping of a scope and size that is in proportion to the scale of the development	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Existing palms and shrubs appear to need to be removed to accommodate construction, however no landscaping is indicated on the plans.
Landscape is designed so that it may be maintained in its natural shape and size	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Preserves a variety of existing native trees and shrubs	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
Provides for a harmonious setting for the site's structures, parking areas or other construction	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	The space is very tight, but the addition of minimal landscaping would add to the project.
Location of existing trees and new trees provides street buffers, mitigation for parking lots, and an architectural complement that visually mitigates between parking lots and building(s)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
Shrubs are selected to complement the natural setting, provide visual interest and screen less desirable elements of the project	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
A variety of species is selected for texture and color	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Provides overall order and continuity of the Landscape plan	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
Native plants or plants that have historically been prevalent on the Island are utilized	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
A variety of sizes is selected to create a "layered" appearance for visual interest and a sense of depth	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
The location of existing mature trees is taken into	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	

account in placement of shrubs so as not to damage tree roots				
Proper spacing and location for plants to reach their mature size and natural shape while avoiding excessive or unnatural pruning	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Proposed groundcovers are evergreen species with low maintenance needs	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Large grassed lawn areas encompassing a major portion of the site are avoided	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
The adjacent development is taken into account in determining the most appropriate buffer so as not to depart too dramatically from the neighborhood	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Ornamentals and Annuals are limited to entrances and other focal points	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	

NATURAL RESOURCE PROTECTION

DESIGN GUIDE/LMO CRITERIA	Complies Yes	No	Not Applicable	Comments or Conditions
An effort has been made to preserve existing trees and under story plants	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
Supplemental and replacement trees meet LMO requirements for size, species and number	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Wetlands if present are avoided and the required buffers are maintained	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Sand dunes if present are not disturbed	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	

MISC COMMENTS/QUESTIONS

Existing site contains minimal plant materials. Proposed addition appears to remove much of existing landscape materials. Staff recommends existing landscaping be retained where possible and replaced if necessary.
Access at the corner of the building is very tight. Applicant will need to work with Staff during DPR process to insure proposed construction does not hinder access to the restaurant or boat storage building and docks.



Design Review Board

Town of Hilton Head Island
Community Development Department
One Town Center Court
Hilton Head Island, SC 29928
Phone: 843-341-4757 Fax: 843-842-8908
www.hiltonheadislandsc.gov

FOR OFFICIAL USE ONLY
Date Received: 7-30-12
Accepted by: SLH
App #: APL 120002
Meeting Date: _____

Applicant/Agent Name: B. Robert Trotter Company: Bob Trotter MD LLC
Patty North
Mailing Address: 18 Hospital Center Blvd City: Hilton Head Is. State: S.C. Zip: 29926
Telephone: (843) 681-9355 Fax: (843) 842-9700 E-mail: DrTrotter@mdvip.com
(843) 842-9600 DrNorth@mdvip.com

APPEAL (APL) SUBMITTAL REQUIREMENTS

- Digital Submissions may be accepted via e-mail by calling 843-341-4757. The following items must be attached in order for this application to be complete:
- A detailed narrative stating the Town Official or Body the made the decision, the date of the decision you are appealing, the decision you are appealing, the basis for your right to appeal, the grounds of the appeal, and citing any LMO Section numbers relied upon; **and** a statement of the specific decision requested of the Board of Zoning Appeals.
 - Any other documentation used to support the facts surrounding the decision.
 - Filing Fee - \$100.00 cash or check made payable to the Town of Hilton Head Island.

To the best of my knowledge, the information on this application and all additional documentation is true factual, and complete. I hereby agree to abide by all conditions of any approvals granted by the Town of Hilton Head Island. I understand that such conditions shall apply to the subject property only and are a right or obligation transferable by sale.

I further understand that in the event of a State of Emergency due to a Disaster, the review and approval times set forth in the Land Management Ordinance may be suspended.

Applicant/Agent Signature: B. Robert Trotter Date: July 30 2012
Patty North

July 30, 2012

The town of Hilton Head Island has issued a "sign violation" against our business located at 18 Hospital Center Blvd.

We are appealing the opinion of the Design Review staff and Jill Foster. As a nationally affiliated business, we have to obtain approval for our sign. We believe that the sign that we designed compliments both the building and the landscaping of our location. Our sign is on a secondary road and sits back approximately 30 feet from the road. It is tucked under a large tree and indigenous foliage. The building is in code with the Design Review Board's approved colors. The sign is complementary to the color of the porch and other exterior elements located on the grounds. The exterior is coordinated with the interior colors used throughout the office.

We would appreciate a variance for our sign as we believe the sign is tasteful and is an example of how an older, established building can have a new and fresh appearance.

Thank you in advance,



Billie Trotter, Office Manager

Trotter/North

18 Hospital Center Blvd

HILTON HEAD ISLAND DESIGN GUIDE



ISLAND CHARACTER VISION STATEMENT

DEVELOPMENT SHALL EXHIBIT A HARMONIOUS RELATIONSHIP WITH THE NATURAL ENVIRONMENT BY BLENDING THE PRINCIPLES OF SENSITIVE SITE PLANNING, SKILLFUL ARCHITECTURAL DESIGN AND AN EMPHASIS ON LANDSCAPING THAT PRESERVES AND ENHANCES THE NATIVE VEGETATION.

**THE GOAL OF THIS DESIGN GUIDE
IS TO PRESERVE THE ISLAND CHARACTER
BY DIRECTING DEVELOPMENT TO:**

- ❖ PRESERVE SIGNIFICANT EXISTING SITE FEATURES, TREES AND VEGETATION.
- ❖ TREAT THE LANDSCAPE AS A MAJOR ELEMENT OF THE PROJECT.
- ❖ PROVIDE LANDSCAPING OF A SCOPE AND SIZE THAT IS IN PROPORTION TO THE SCALE OF THE DEVELOPMENT.
- ❖ DESIGN AND MAINTAIN LANDSCAPING IN ITS NATURAL SHAPE AND SIZE.
- ❖ DESIGN STRUCTURES APPROPRIATE FOR THEIR USE AND NEIGHBORHOOD.
- ❖ PROMOTE PEDESTRIAN SCALE AND CIRCULATION.

- ❖ DEMONSTRATE THE FUNDAMENTAL PRINCIPLES OF GOOD ARCHITECTURAL DESIGN.
- ❖ DESIGN STRUCTURES WITH SUBTLE VISUAL IMPACT AND UTILIZE NATURAL MATERIALS, TEXTURES AND COLORS.
- ❖ PROVIDE LIGHTING THAT IS ADEQUATE FOR SAFETY AND ENHANCES THE SITE.
- ❖ COORDINATE AND HARMONIZE THE DESIGN OF STRUCTURES, PARKING, AND SITE AMENITIES.
- ❖ PROVIDE CONTINUITY OF DESIGN ON ALL FACADES OF THE BUILDING.
- ❖ CONCEAL VISUALLY UNDESIRABLE UTILITIES AND EQUIPMENT.

ACCESSORY CONSTRUCTION

Thought must also be given to the design and placement of other elements that may be part of a project with the aim being to achieve overall coordination. These may include signs, awnings, sculpture & fountains, lighting and utilities and equipment.



Signs. Signs are an important element to most commercial buildings and developments. Too often no allowance is made in the design of projects for the placement of such signs. Many of the principles for good design of structures also apply to signs. Sign design does not begin with a blank sheet of paper. The materials, details and colors of the building are all starting points for the sign design. In other words, signs should reflect the design of the project they are intended to identify.

The size and number of signs will be determined by LMO Chapter 5, Article XIII. Signs should serve to identify the business or development and not act as advertisements. Tenant signs should be uniform in design and placed on the façade of the



tenant space. Lighting, if used, must be completely shielded from streets and pathways. Bright colors and reflective surfaces should be avoided or very limited in size.

Freestanding or monument signs should be placed in logical locations near the project's entrance drive. They should provide strong visual interest, three dimensional design and high quality, durable construction. Vinyl copy on painted plywood supported by 4X4 wood posts does not demonstrate Island Character. Adequate landscaping must be provided to blend the sign into the site. For façade signs adequate wall space must be provided in a location that will allow the sign to function properly while also appear as if it "belongs" with the building.



*Trotter North
14-Hospital Center Blvd*

PURPOSE

SEC 16-5-1301

IT IS THE PURPOSE OF THIS ARTICLE TO PROMOTE THE PUBLIC HEALTH, SAFETY AND GENERAL WELFARE THROUGH A COMPREHENSIVE SYSTEM OF REASONABLE, CONSISTENT AND NONDISCRIMINATORY SIGN STANDARDS AND REQUIREMENTS. THESE SIGN REGULATIONS ARE INTENDED TO:

- A. MAXIMIZE THE VALUE OF COMMERCIAL SIGNAGE AS A MEANS OF LOCATING AND IDENTIFYING COMMERCIAL ESTABLISHMENTS PROVIDING GOODS AND SERVICES, WHILE, AT THE SAME TIME, DISCOURAGING THE USE OF COMMERCIAL SIGNAGE TO SELL GOODS AND SERVICES.
- B. ENCOURAGE THE CONSTRUCTION OF COMMERCIAL SIGNS OF HIGH-QUALITY MATERIALS WHICH ARE AESTHETICALLY PLEASING AND ARE COMPATIBLE WITH THEIR NATURAL SURROUNDINGS AND WITH THE BUILDINGS THEY IDENTIFY.
- C. AVOID THE CREATION OF A DISTRACTING ATMOSPHERE WHICH CAN RESULT WHEN BUSINESSES COMPETE FOR ATTENTION THROUGH THE USE OF COMMERCIAL ADVERTISING SIGNS.
- D. PROTECT, PRESERVE AND ENHANCE THE UNIQUE AESTHETIC CHARACTER, BEAUTY AND CHARM OF THE TOWN, AND THEREBY ENCOURAGE THE CONTINUED ECONOMIC DEVELOPMENT WITHIN THE TOWN.
- E. IMPROVE PEDESTRIAN AND TRAFFIC SAFETY AND ELIMINATE PHYSICAL AND VISUAL CLUTTER CAUSED BY SIGNS WHICH COMPETE FOR THE ATTENTION OF PEDESTRIAN AND VEHICULAR TRAFFIC.

APPLICABILITY AND PROVISIONS

SEC 16-5-1302

- A. A SIGN MAY BE ERECTED, PLACED, ESTABLISHED, PAINTED, CREATED OR MAINTAINED IN THE TOWN ONLY IN CONFORMANCE WITH THE STANDARDS, PROCEDURES, EXEMPTIONS AND OTHER REQUIREMENTS OF THIS TITLE. SIGNS EXEMPT FROM REGULATIONS UNDER SEC. 16-5-1322 SHALL NOT OTHERWISE BE SUBJECT TO THIS TITLE.
- B. WITH THE EXCEPTION OF SEC. 16-5-1313 AND SEC. 16-5-1314, THE PROVISIONS OF THIS TITLE SHALL NOT APPLY TO SINGLE FAMILY USES, AS DEFINED AND DESCRIBED ELSEWHERE IN THIS TITLE.
- C. SIGNS LOCATED ON PROPERTY WITHIN THOSE PORTIONS OF PD-1 DISTRICTS WHERE VEHICULAR ACCESS BY THE GENERAL PUBLIC IS RESTRICTED BY A SECURITY GATE STAFFED TWENTY FOUR (24) HOURS EACH DAY BY A SECURITY GUARD AND WHERE SUCH SIGNS ARE NOT VISIBLE FROM ANY BEACH OR NAVIGABLE WATERWAY ARE NOT SUBJECT TO THE PROVISIONS OF THIS TITLE.
- D. NOTWITHSTANDING ANY OTHER PROVISION OF THIS ARTICLE, NO SIGN SHALL BE SUBJECT TO ANY LIMITATION BASED ON THE CONTENT OF THE MESSAGE CONTAINED ON SUCH SIGN.
- E. SEVERABILITY PROVISION. IF ANY PART, SECTION, SUBSECTION, PARAGRAPH, SUBPARAGRAPH, SENTENCE, PHRASE, CLAUSE, TERM, OR WORD OF THIS ARTICLE AND/OR ANY OTHER CODE PROVISIONS AND/OR LAWS ARE DECLARED INVALID OR UNCONSTITUTIONAL BY ANY COURT OF COMPETENT JURISDICTION, THE DECLARATION OF SUCH UNCONSTITUTIONALITY SHALL NOT AFFECT ANY OTHER PROVISION CONTAINED HEREIN.
- F. ANY LEGAL SIGN THAT DOES NOT COMPLY WITH THE PROVISIONS OF THIS ARTICLE DUE SOLELY TO THE ENACTMENT OF AN AMENDMENT SHALL, UPON THE EFFECTIVE DATE OF SUCH AMENDMENT, BECOME A NONCONFORMING SIGN AND SHALL BE SUBJECT TO THE PROVISIONS OF CHAPTER 7.

- G. ALL SIGNS SHALL COMPLY WITH APPLICABLE BUILDING AND ELECTRICAL CODE REQUIREMENTS.
- H. PER INTERNATIONAL BUILDING CODE (IBC) SECTION 1609.1, APPLICATIONS FOR NEW FREESTANDING SIGNS WITH FACES LARGER THAN FORTY (40) SQUARE FEET SHALL INCLUDE WIND LOAD CALCULATIONS STAMPED AND SIGNED BY A CERTIFIED ENGINEER STATING THAT THE SIGN CAN WITHSTAND WINDS OF UP TO 130 MILES PER HOUR.
- I. SUBSTITUTION OF NONCOMMERCIAL MESSAGE. NONCOMMERCIAL SIGNS SHALL BE ALLOWED IN ALL ZONING DISTRICTS AND MAY BE SUBSTITUTED FOR ANY SIGN EXPRESSLY ALLOWED UNDER THIS ORDINANCE. NONCOMMERCIAL SIGNS SHALL BE SUBJECT TO THE SAME PERMIT REQUIREMENTS, RESTRICTIONS ON SIZE AND TYPE, AND OTHER CONDITIONS AND SPECIFICATIONS AS APPLY TO THE SIGN FOR WHICH THEY ARE BEING SUBSTITUTED.

SIGN DESIGN, CONSTRUCTION, AND MAINTENANCE GUIDELINES

Sec. 16-5-1303

THE HILTON HEAD ISLAND DESIGN GUIDE DEFINES ISLAND CHARACTER AND DESCRIBES HOW DEVELOPMENT SHOULD BE DIRECTED TO PRESERVE ISLAND CHARACTER. GOALS OF THE DESIGN GUIDE THAT PERTAIN TO SIGN DESIGN INCLUDE:

- A. DEMONSTRATE THE FUNDAMENTAL PRINCIPLES OF GOOD ARCHITECTURAL DESIGN.
- B. DESIGN STRUCTURES WITH SUBTLE VISUAL IMPACT AND UTILIZE NATURAL MATERIALS, TEXTURES AND COLORS.
- C. COORDINATE AND HARMONIZE THE DESIGN OF STRUCTURES, PARKING AND SITE AMENITIES.
- D. CONCEAL VISUALLY UNDESIRABLE UTILITIES AND EQUIPMENT.

TO MEET THESE GOALS, SIGN DESIGN, CONSTRUCTION AND MAINTENANCE SHALL MEET THE FOLLOWING STANDARDS:

- E. MATERIALS, COLORS, AND SHAPES OF PROPOSED SIGNS SHALL BE COMPLEMENTARY TO THE RELATED BUILDINGS AND TO NEARBY STRUCTURES AND SIGNS. SIGN COLORS SHALL BE NON-REFLECTIVE AND SHALL NOT CONTAIN FLUORESCENT COLORS. SEE **EXAMPLES OF SIGNS THAT ARE COMPLEMENTARY TO NEARBY STRUCTURES** BELOW.
- F. SIGNS SHALL BE CONSTRUCTED OF HIGH-QUALITY MATERIALS. SEE DESIGN GUIDE, PAGE 13.
- G. THE AMOUNT OF INFORMATION ON SIGNS SHALL BE NO MORE THAN IS NECESSARY TO PROVIDE REASONABLE IDENTIFICATION OF THE BUSINESS OR OF ANY MESSAGE TO BE CONVEYED. OBJECTIVE FACTORS TO CONSIDER IN DETERMINING REASONABLENESS SHALL INCLUDE, BUT NOT BE LIMITED TO, PHYSICAL LOCATION, SIZE OF THE SIGN AND TYPEFACE, COLORS OF THE SIGN AND TYPEFACE AND SIGN HEIGHT.
- H. THE VISUAL IMPACT OF FREESTANDING SIGNS SHALL BE SOFTENED WITH LANDSCAPING APPROPRIATE TO THE SITE.
- I. SIGN ILLUMINATION SHALL MEET THE STANDARDS IN SEC. 16-5-1304.
- J. SIGNS SHALL BE MAINTAINED IN GOOD CONDITION AT ALL TIMES AND SHALL BE KEPT FREE OF CRACKED OR PEELING PAINT, MISSING OR DAMAGED SIGN PANELS OR SUPPORTS, AND WEEDS, GRASS OR VEGETATION THAT OBSCURES THE VIEW OF THE SIGN MESSAGE.



TOWN OF HILTON HEAD ISLAND

Community Development Department

TO: Design Review Board
VIA: Teri Lewis, AICP, *LMO Official*
FROM: Jennifer Ray, *Urban Designer*
DATE: August 7, 2012
SUBJECT: Appeal 120002

Staff has received an appeal from Patty North, MD LLC and Robert Trotter, MD LLC regarding the June 22, 2012 letter stating the freestanding sign at 18 Hospital Center Boulevard does not match the approved design. The appellant is appealing the Town's decision to not approve a non-permitted background color on the sign face and is asking that the Board reverse the decision of Town staff and approve the background color.

The appellants' agent, Howard Wright of Hilton Head Signs, submitted a sign permit application on May 9, 2012. The application included a rendering of the proposed sign; the background color of the sign face was a color equivalent to Pantone 331. Staff determined that color was too bright and instead suggested Pantone 5425. The revised rendering was submitted to staff and it was approved on May 11, 2012.

When staff made a site visit to take a compliance photo of the sign, it was clear that the sign did not match the approved design; the background color was the equivalent of Pantone 331. Staff determined that the background color does not meet the intent of the Town of Hilton Head Island Design Guide, which states that colors should be nature-blending and reflect the design of the associated structure. Specifically, on the page 15 the Design Guide states "The materials, details and colors of the building are all starting points for the sign design. In other words, signs should reflect the design of the project they are intended to identify." Also, on page 15 the Design Guide states "Bright colors and reflective surfaces should be avoided or very limited in size." The sign's background color does not reflect any color clearly visible on the exterior of the building and the background color is bright, both of which are contrary to the intents of the Design Guide.

Staff contacted Mr. Wright to discuss the sign. Mr. Wright stated that he was aware that the background color did not match the approved color, but that he painted the sign background Pantone 331 at the direction of his client. Mr. Wright directed staff to contact his client via Jeffrey North, LLC.

Staff sent a letter to Mr. North on June 22, 2012 stating that the sign was not compliant with the approved design and therefore was in violation of the Town of Hilton Head Island Land Management Ordinance (LMO) Section 16-5-1302A, which states that "A sign may be erected, placed, established, painted, created or maintained in the Town only in conformance with the standards, procedures, exemptions and other requirements of this Title [LMO]." The letter gave two

options to resolve the violation: submit an alternative background color or colors to staff for review; or to submit an application to the Design Review Board for review of the existing sign.

Mr. North contacted staff to discuss the letter and requested that staff reconsider approving PMS 331 as the background color. Staff explained to Mr. North the reasoning behind the decision and informed him that the background color did not have to be Pantone 5425 (as approved); staff would consider other background colors that met the intent of the Town's Design Guide.

Mr. North requested that staff meet with Patty North and Billie Trotter on site to discuss the sign. On July 5, 2012, staff met with Ms. North and Ms. Trotter, who stated the background color was mandated by their parent company (MD VIP) and stated the color matched the interior design of the facility and the porch roof. Staff stated that they have worked with several businesses (McDonald's, Dunkin' Donuts) with franchise or corporate color restrictions and have been able to approve signs with toned-down color palates. To that end, staff suggested a more neutral background color (such as one of the beige colors on the building) with Pantone 331 as an accent color. Ms. North and Ms. Trotter stated they did not want to change the background color and said they would apply for Design Review Board Approval.

On July 9, 2012, Mr. North requested that staff extend the deadline to resolve the sign violation. On July 10, 2012, staff sent a letter to Mr. North extending the deadline from July 13, 2012 to August 10, 2012. The letter stated the same options to resolve the violation were still available: submit an alternative background color or colors to staff for review; or to submit an application to the Design Review Board for review of the existing sign.

On July 23, 2012, Ms. Trotter met with staff to discuss the sign. Ms. Trotter stated that she had painted the colors of the copy brown to tone down the sign's color. Staff informed Ms. Trotter that the sign's background color must be toned down, but the copy could remain Pantone Cool Grey 1 as approved. Staff suggested a few alternative background colors, but Ms. Trotter stated that she did not want to change the background color. Staff informed Ms. Trotter that she should apply to the Design Review Board since she did not want to change the background color and staff would not approve the background color.

On July 24, 2012, Ms. Trotter sent an email to staff stating that she would submit an appeal to the Design Review Board. On the same day, Mr. North contacted staff to discuss a way to resolve the sign issue. Again staff suggested some alternative background colors that would meet the intent of the Design Guide and informed Mr. North that Ms. Trotter's options are to select and alternate background color or appeal to the Design Review Board at its August 14, 2012 meeting.

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PATTY NORTH, M.D.

ROB TROTTER, M.D.

Internal Medicine

MDVIP 